



a division of Englobe

Draft Report for:

DISTRICT OF
OAK  BAY

SIDEWALK AND CURB ASSESSMENT REPORT

Date: May 3, 2024
Project No. 9404-001-00

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May 3, 2024

The Corporation of the District of Oak Bay
2167 Oak Bay Avenue
Victoria, BC, V8R 1G2

Attention: Richard Ding, M.Sc., P.Eng.

Dear Richard:

Re: Sidewalk and Curb Assessment Report 2024

MPE a division of Englobe (MPE) is pleased to submit an electronic copy of our Report entitled *Sidewalk and Curb Assessment Report 2024*.

Should you have any questions, or require clarification on any item, please do not hesitate to contact the undersigned at 778-231-8880 or at Jculp@mpe.ca

Yours truly,

MPE a division of Englobe



Joshua Culp, ASCT, P.L.Eng.
Project Manager

Enclosure.

CORPORATE AUTHORIZATION

This report has been prepared by MPE a division of Englobe under authorization of The District of Oak Bay. The material in this report represents the best judgment of MPE a division of Englobe given the available information. Any use that a third party makes of this report, or reliance on or decisions made based upon it is the responsibility of the third party. MPE a division of Englobe accepts no responsibility for damages, if any, suffered by a third party as a result of decisions made or actions taken based upon this report.

Should any questions arise regarding content of this report, please contact the undersigned.

MPE a division of Englobe

Professional Stamp



Joshua Culp, ASCT, P.L. Eng.

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Professional Seal

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EXECUTIVE SUMMARY

In 2023 the District of Oak Bay (District) retained the services of MPE a division of Englobe (MPE) to undertake a comprehensive sidewalk and curb evaluation program. The purpose of the assessment is the collection of detailed sidewalk and curb condition and performance data as part of the District’s infrastructure management program. The program requires that approximately 111 km of the District’s sidewalk network, and 162 km of the curb network be inventoried, with the surveys being conducted in early 2024. **The District budgets \$500K per year for sidewalk, curb and gutter capital projects and \$135K for maintenance.**

The breakdown of the data collection and reporting program, conducted in the District, is as follows:

- ❑ GIS networks (sidewalk and curb layers) updates and integration.
- ❑ Detailed sidewalk condition inspection (111 kilometres).
- ❑ Detailed curb condition inspection (162 kilometres).
- ❑ Performance and maintenance analysis.
- ❑ Delivery of geospatial databases containing all inspections and performance scores.
- ❑ Preparation and delivery of the network condition assessment report.

The data collected during the field inspections was used to identify the condition of the networks in terms of the ASTM PCI (pavement condition index) inspection guidelines:

- ❑ Concrete sidewalks – modified ASTM methodology for concrete (PCC)
- ❑ Asphalt sidewalks – modified ASTM methodology for asphalt (AC)
- ❑ Curb network – separate visual condition assessment (Good, Fair, or Poor)

For the purpose of clarity, in this report, the sidewalk modified ASTM scores are **termed SCI (Sidewalk Condition Index)**.

SIDEWALK NETWORK CONDITION

The present status of the District’s sidewalk network is summarized in **Table ES.1**.

Table ES.1: Sidewalk Network Performance Summary

| FACILITY | SEGMENTS | LENGTH (KM) | #PANELS | SCI |
|------------------|----------|-------------|---------|-----------|
| Sidewalk Network | 816 | 110.7 | 31,968 | 76 |

The following observations and conclusions are drawn from the sidewalk survey and analysis:

- ❑ The mean SCI of 76 indicates that overall, the sidewalk network is in satisfactory condition with respect to the defects present.
- ❑ A majority (64%) of the network is in satisfactory or good condition with SCI scores above 70.

- ❑ The results indicate that scaling is the most common recorded distress with 8,942 slabs affected throughout the network.
- ❑ The network included 14.9 km of asphalt sidewalks with an overall satisfactory condition.
- ❑ The critical conditions summary identified the following:
 - 74 trip hazards at the moderate or high severity level
 - 186 missing or failed para ramps
 - 1,003 observations of high severity distresses
- ❑ The maintenance results show that slab replacement is the most common recommended treatment.
- ❑ Budget analysis results indicate a total cost of \$9.9M is required in the sidewalk network. Capital projects will require \$3.2M (32%) and maintenance programs will require \$6.7M (68%). The high slab replacement costs are due the Districts policy of replacing slabs where the scaling distress has reached the moderate or severe level, and where adjacent curb replacements are required.
- ❑ There are 2,352 sidewalk slabs identified as having moderate-to-severe scaling, of which 472 (20%) are severe. The district may want to consider a ‘lighter’ treatment option (e.g. surface repair) for moderately scaled sidewalk slabs to reduce the replacement cost burden on the available budget.

CURB NETWORK CONDITION

The present status of the District’s curb network is summarized in **Table ES.2**.

Table ES.2: Curb Network Performance Summary

| FACILITY | SEGMENTS | LENGTH (KM) | CONDITION |
|--------------|----------|-------------|-------------|
| Curb Network | 1,121 | 162.3 | Fair |

The following observations and conclusions are drawn from the curb survey and analysis:

- ❑ The assessment of the curb network indicates an overall fair condition with respect to the defects present.
- ❑ A review of the distress results indicates that raveling and linear cracking are the most observed defects in the curb network. Most occurrences of these distresses are at the low severity level.
- ❑ The critical conditions summary identified the following:
 - 15,202 meters of curb will require replacement largely due to adjacent sidewalk replacement needs and paved gutters causing poor curb height conditions.
 - 464 segments have pavement overlays encroaching into the gutter. In the case of 53 of these segments the paved gutters caused the curb condition to be Poor.
- ❑ Budget analysis results indicate a total cost of \$8.7M is required in the curb. Capital projects will require \$4.5M (52%) and maintenance programs will require \$4.2M (48%).

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1.0 PROJECT OVERVIEW

The District of Oak Bay (District) is responsible for the administration and maintenance of an important infrastructure network consisting of sidewalk and curb facilities. These infrastructure networks form valuable assets to be managed in a cost-effective manner, in order to provide a desirable level of service to the community stakeholders. The District is implementing an ongoing infrastructure assessment program to manage the performance and capital planning of these networks. **The District budgets \$500K per year for sidewalk, curb, and gutter capital projects and \$135K for maintenance.**



1.1 Scope and Objectives

In 2023 the District retained the services of MPE a division of Englobe (MPE) to undertake a comprehensive sidewalk and curb evaluation program. The purpose of the assessment is the collection of detailed sidewalk and curb condition and performance data as part of the District's infrastructure management program. The program requires that approximately 111 km of the District's sidewalk network, and 162 km of the curb network be inventoried, with the surveys being conducted in early 2024.

The breakdown of the data collection and reporting program, conducted in the District, is as follows:

- ❑ GIS networks (sidewalk and curb layers) updates and integration.
- ❑ Detailed sidewalk condition inspection (111 kilometres).
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- ❑ Delivery of geospatial databases containing all inspections and performance scores.
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The data collected during the field inspections was used to identify the condition of the networks in terms of the ASTM PCI (pavement condition index) inspection guidelines:

- ❑ Concrete sidewalks – modified ASTM methodology for concrete (PCC)
- ❑ Asphalt sidewalks – modified ASTM methodology for asphalt (AC)
- ❑ Curb network – separate visual condition assessment (Good, Fair, or Poor)

For the purpose of clarity, in this report, the sidewalk modified ASTM scores are **termed SCI (Sidewalk Condition Index)**.

Based on the calculated sidewalk SCI scores, a performance classification level is assigned using an established range of values. The ASTM guidelines and definitions are an industry standard; hence the distress ratings and condition classifications are defined as per the ASTM D6433 standards for concrete and asphalt inspections.

Figure 1.1 illustrates the ASTM standard distress classifications and corresponding PCI/SCI ranges.

| Distress Rating | PCI Range |
|-----------------|-----------|
| Good | 86 – 100 |
| Satisfactory | 70 – 85 |
| Fair | 56 – 70 |
| Poor | 41 – 55 |
| Very Poor | 26 – 40 |
| Serious | 11 – 25 |
| Failed | 0 – 10 |

Figure 1.1 ASTM Standard PCI™ Condition Classification

Over time, climate and weather, usage, and aging cause infrastructure to deteriorate. Maintenance and/or rehabilitation options applied at the appropriate time can renew and extend the life of these municipal networks. The objective of infrastructure asset management is to maximize the present and future value, and level of service, of these important pedestrian networks by cost-effective management of available public capital funds. The ASTM based standards provide network administrators with the tools to evaluate, monitor, and report on the condition of their networks, using industry proven and repeatable methodologies.

The separate curb condition inspections were conducted as a visual assessment during the sidewalk survey. Where curb structures were independent of the sidewalk network, digital imagery collected in the field was used to assess the conditions. The curb conditions were based on an overall Good, Fair, Poor ranking.

Figure 1.2 and **Figure 1.3** on the following pages show the sidewalk and curb survey coverage.

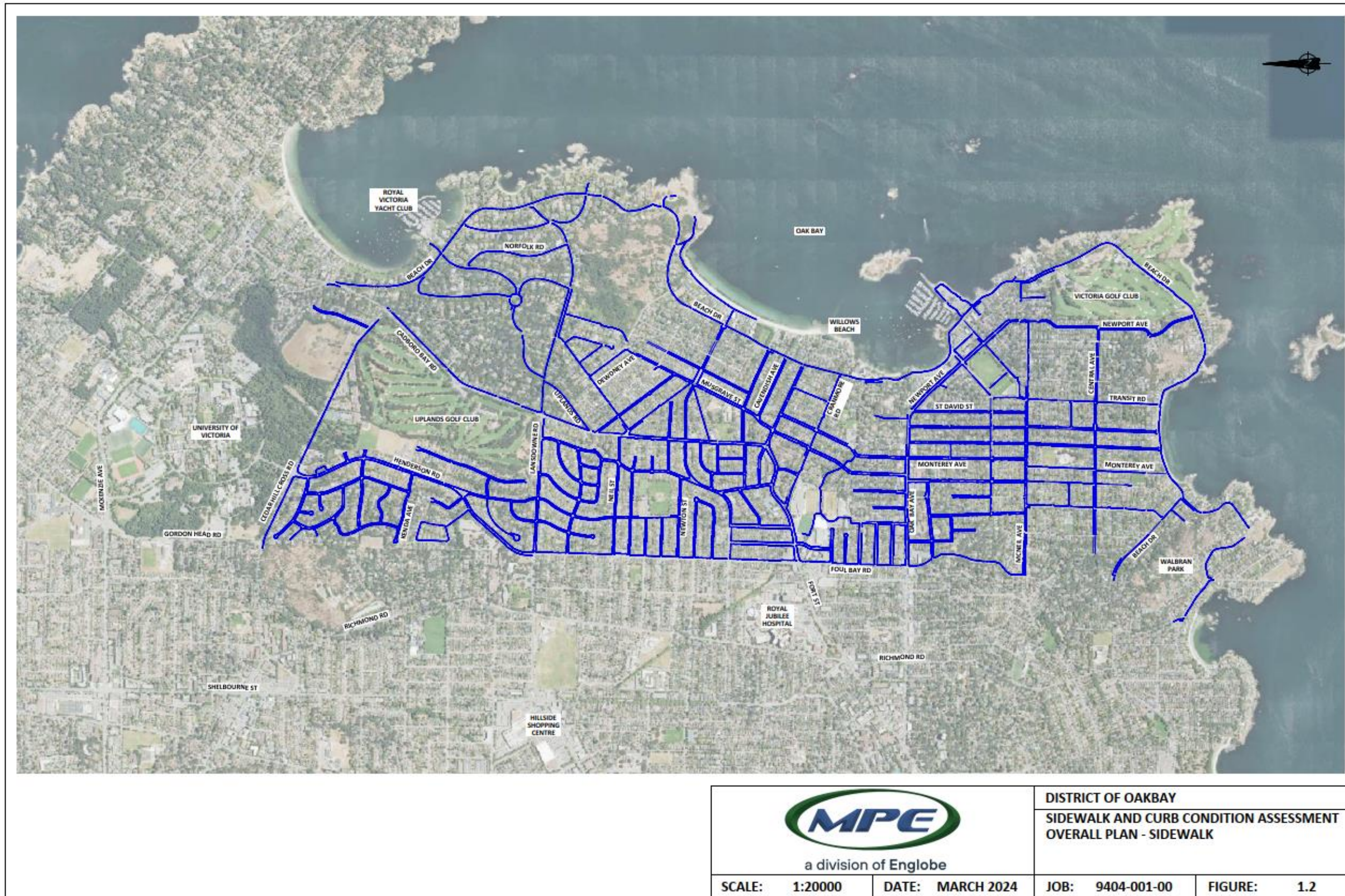


Figure 1.2: Sidewalk Survey Coverage – 2024

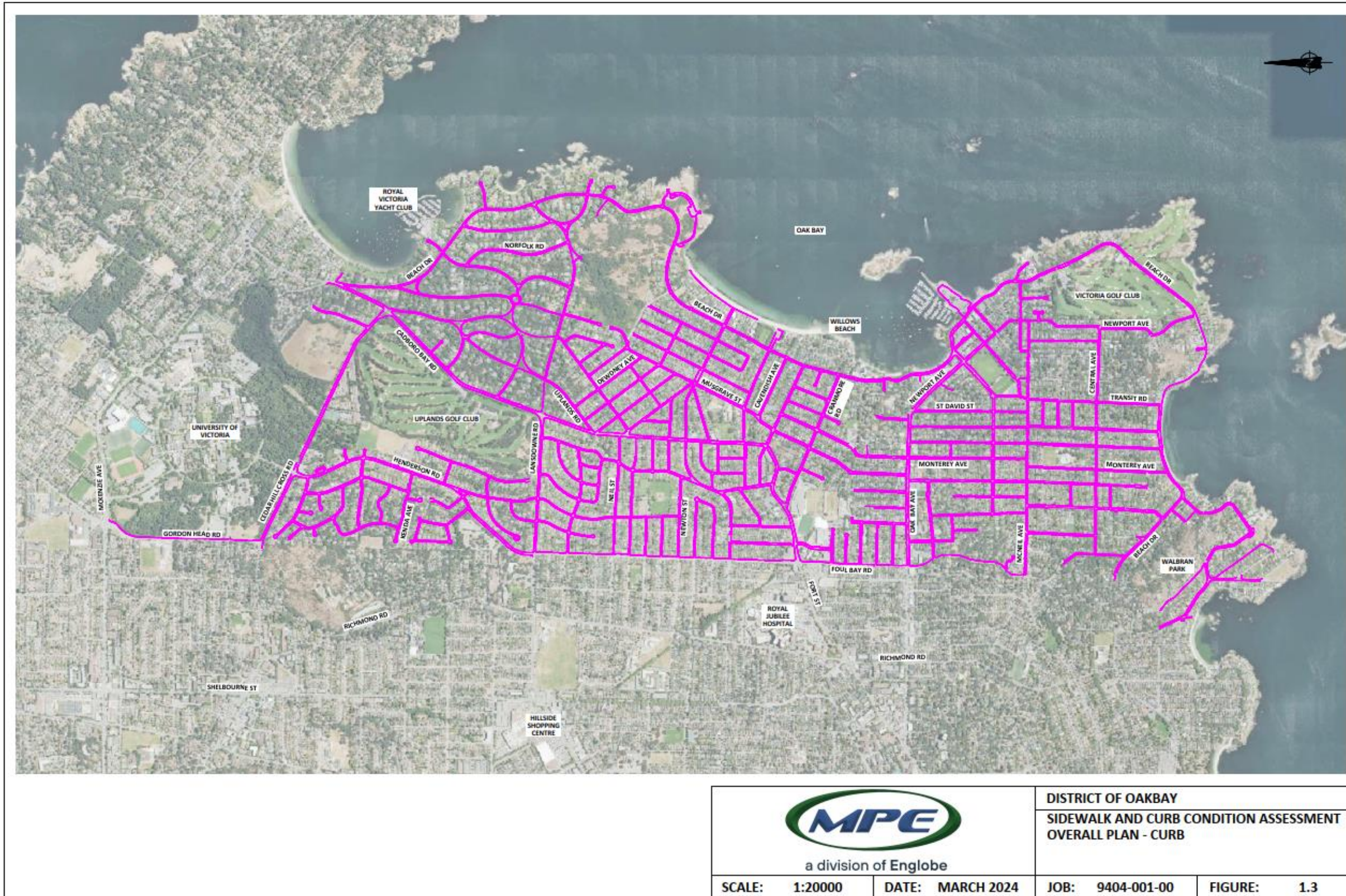


Figure 1.3: Curb Survey Coverage – 2024

2.0 FIELD INSPECTIONS

2.1 GIS Network Definitions

The network definition and attribute data setup consisted of the following:

- ❑ Establish the survey coverage using the District’s updated sidewalk and curb GIS layers, maintaining a direct link to the unique Asset ID for each network segment.
- ❑ Activate and load the RUBIX asset management framework (rDash).
- ❑ Classify network attributes used for analysis (user exposure, geometrics, etc.).

The sidewalk and curb network definitions, used for the purpose of the report, were implemented based on GIS databases provided by the District and updated by MPE. Final Asset ID updates were made during data processing, and some modifications were made to the network attributes based on actual conditions encountered during the field surveys.

2.2 Data Collection Framework



MPE utilizes the **RUBIX rInspector** application, loaded to our iPad devices, and synchronized with our RUBIX Dashboard framework. The iPad devices are set up with customized data entry forms, to record the location, type, severity and extent of each defect identified in the survey scope. In addition, MPE will take georeferenced images of various defects and input relevant notes at locations of special interest.

Customized GPS enabled forms were developed to collect the type, severity and extent of the hazards, distresses, and observations encountered in the field. The rInspector forms were built to enable the efficient capture of the condition data for three main material types found in pedestrian networks. These material types are concrete (PCC), asphalt (AC) and brick pavers, with each material type requiring different capture techniques and summary calculations.

Field inspections were conducted as walking surveys, with measurements taken at each defect location. In cases where sidewalks were not present, curbs assessments were conducted from digital imagery captured separately. All georeferenced inspections and images are automatically spatially related to the appropriate sidewalk or curb segment, using the geospatial analytics of the RUBIX Dashboard framework.

MPE applied ASTM-based methodologies to the concrete and asphalt sidewalk inspections. The curb condition assessment was done visually at the segment level and assigned an overall condition of Good, Fair, or Poor.

2.3 Sidewalk Network

The District’s sidewalk network is constructed from two distinct material types: concrete slabs and asphalt surfaces. For the purpose of the assessment, MPE employed two different ASTM data collection and analysis models to classify the conditions of the respective portions of the sidewalk network. The breakdown of the network, between the material types, is **concrete 95.8 km and asphalt 14.9 km**.

2.3.1 Concrete (PCC) Methodology

The inspections were conducted over the full concrete portion of the defined sidewalk network, recording the occurrence of defects at each affected slab. In the field, additional data is collected for geometrics, condition of para-ramps, and curbs. The inspection logged the presence and severity of trip hazards and obstructions, as well as the extent (number of slabs affected), and severities for the main distress types affecting concrete sidewalk structures.

The following distress types, observations and hazards were inventoried:

| CONCRETE DISTRESS TYPES AND OTHER CONDITIONS | | |
|--|---------------------------------|--------------------|
| PCC Distress Ratings (9) | Observation Classifications (4) | Hazard Ratings (1) |
| Small Patch | Vegetation Encroachment | Trip/Slip Hazard |
| Large Patch | C-G Type, Material, Condition | |
| Scaling | Para-Ramp Condition | |
| Shattering | Obstructions | |
| Corner Break | | |
| Linear Crack | | |
| Corner Spalling | | |
| Joint Spalling | | |
| Faulting | | |

The detailed concrete data collection methodology was provided to the District in a separate document titled ***Inspection Manual for: District of Oak Bay – Sidewalk and Curb Condition Assessment***.

MPE applied the established nominal slab length of 3 m, as defined by the specifications, for the purpose of the data collection and project analysis.

2.3.2 Asphalt (AC) Methodology

The asphalt surface inspections recorded the location, severity, and quantity of each distress present. The detailed inspection data is summarized by sidewalk segment, and the resulting SCI scores will be produced at this summary level.

The following distress types, observations and hazards were inventoried:

| ASPHALT DISTRESS TYPES AND OTHER CONDITIONS | | |
|--|--|---------------------------|
| AC Distress Ratings (7) | Observation Classifications (4) | Hazard Ratings (1) |
| Patching | Vegetation Encroachment | Trip/Slip Hazard |
| Raveling | C-G Type, Material, Condition | |
| Distortion (Bumps and Sags) | Para-Ramp Condition | |
| Alligator Cracking | Obstructions | |
| Edge Cracking | | |
| Linear Cracking | | |
| Potholes | | |
| | | |

The detailed asphalt data collection methodology was provided to the District in a separate document titled *Inspection Manual for: District of Oak Bay – Sidewalk and Curb Condition Assessment*.

2.4 Curb Network

The curb condition assessment was done as an overall ranking assessment at the segment level (Good, Fair, or Poor). In addition, individual locations within a segment where notable defects were present were identified.

The curb data collection methodology was provided to the District in a separate document titled *Inspection Manual for: District of Oak Bay – Sidewalk and Curb Condition Assessment*.

3.0 CONDITION ASSESSMENTS

3.1 Network Analysis

3.1.1 Surface Defects – Sidewalk Condition Index (SCI) Analysis

The detailed sidewalk inspection data is analyzed to produce key performance indicators for each station and sidewalk segment. The main indicator is the Sidewalk Condition Index (SCI) score, which is calculated based on the ASTM methodology.

Modified ASTM Deduct Value (DV) models are assigned to each sidewalk defect type. These models calculate the individual defect deduct scores. Multiple defect scores are combined using further models to produce Adjusted Deduct Values. The resulting scores are referred to as the Distress Index (DI) and are reported on a 10-scale, or converted to a 100-scale, where the higher the value the more severe the distress condition.

The Sidewalk Condition Index (SCI) score is calculated as a function of the Distress Index (DI) score and is reported on a 100-scale.

As part of the defect analysis, the Hazard Index (HI) is calculated from the aggregation of the individual Trip Hazards identified for each sidewalk segment. A separate DV model is used to calculate the HI based on the trip hazard inspections. The results of the HI analysis determine whether a segment is considered to have a Low, Moderate, or High hazard level.

The detailed *ASTM PCI Analysis Methodology* is provided in *Appendix A*.

3.1.2 Pedestrian Exposure – Pedestrian Index (PI) Analysis

The pedestrian exposure analysis takes into consideration the importance/placement of the sidewalk structure, the level of pedestrian traffic (usage), and most importantly, the criticality of the pedestrian hazards (trip and slip) identified in the field inspections. Each of these three influence factors are summarized as Low, Medium, or High for each unique block-face.

The combination of the influence factors analyzed through a criticality matrix produces a sidewalk Pedestrian Index (PI) based on a 10-scale, where the higher the index score the more critical the asset from a pedestrian exposure perspective.

Figure 3.1 illustrates the Pedestrian Exposure matrix used for the analysis.

| PEDESTRIAN INDEX | | Importance/Context | | | | | | | | |
|------------------|------|-----------------------|-----|-----|------|-----|-----|------|-----|-----|
| | | High | | | Med | | | Low | | |
| | | Pedestrian Generation | | | | | | | | |
| | | High | Med | Low | High | Med | Low | High | Med | Low |
| Urgency/Severity | High | 10 | 10 | 10 | 9 | 9 | 8 | 9 | 8 | 7 |
| | Med | 8 | 8 | 7 | 7 | 6 | 5 | 6 | 5 | 4 |
| | Low | 6 | 5 | 5 | 4 | 4 | 3 | 3 | 2 | 1 |

Figure 3.1 Pedestrian Exposure Matrix

The Pedestrian Index is a function of the following:

- ❑ **Urgency/Severity** (Hazard Condition) – assessed based on the Hazard Index (HI) calculation from the field inspections.
- ❑ **Importance/Context** (Placement) – assessed based on frontage or zoning (e.g., Retirement Home, Hospital, School, etc.).
- ❑ **Pedestrian Generation** (Usage) – assessed based on the pedestrian traffic (e.g., CBD, retail corridors, residential neighborhoods etc.).

3.1.3 Prioritization – Priority Value (PV) Analysis

Once the pedestrian exposure has been defined, a Priority Value (PV) is calculated. The PV differentiates the sidewalk network based on urgency. It is calculated as a function of the DI and the PI. It provides a numerical value representing the combined sidewalk condition and pedestrian exposure level. When sorted from highest to lowest it produces a priority ranking from most urgent to least urgent.

Sidewalk prioritization lists are created by sorting the network based on a combination of summary indicators, reflecting the objectives of the District. Typical priority sorts are based first on the HI (highest-to-lowest), and then by the PV (highest-to-lowest).

In consultation with District staff MPE implemented a customized methodology to prioritize the maintenance in the sidewalk network. The prioritization involved several factors including trip hazards, failed or missing para ramps, and needs in the curb network.

The prioritization methodology is summarized as follows:

- ❑ **First Priority** – Segments with trip hazards are sorted from the highest severity to the lowest severity.
- ❑ **Second Priority** – Segments with missing or failed para ramps are sorted by highest number of affected locations to the lowest occurrences.
- ❑ **Third Priority** – Segments that are adjacent to curbs that require replacements largely due to pavement overlays that have encroached into the gutter causing a low curb reveal condition.
- ❑ **Fourth Priority** – All remaining segments are sorted by project size (highest to lowest) and priority index (highest to lowest).

The staged approach to the prioritization is designed to elevate the priority of maintenance where public safety and accessibility are impacted the most.

The following explains several examples of segments at different rankings in the prioritization:

- ❑ **First Priority Group** – Segment SDWK_576N Satellite St (Poor condition) is ranked 1 on the priority list as it has the highest Hazard Index of 4.71. This is an example of the prioritization of user safety.

- ❑ Second Priority Group – Segment SDWK_128W Cardiff Pl (Fair condition) is ranked 76 on the priority list as it has no hazards but has the highest number (3) of missing/failed para ramps. This is an example of the prioritization of user accessibility.
- ❑ Third Priority Group – Segment SDWK_484E Newport Ave (Serious condition) is ranked 220 on the priority list as it has no hazards or missing/failed ramps. This segment has adjacent curb replacement requirements and the highest Priority Index (31.73) of this group. This is an example of the prioritization for replacement needs in the curb network due to low curb height as a result of paved gutters.
- ❑ Fourth Priority Group – Segment SDWK_153W Christie Way (Satisfactory condition) is ranked 450 on the priority list as it has no hazards, missing/failed ramps, or adjacent curb needs. It has a lower project cost and a low Priority Index (6.5). This is an example of the remaining prioritization for maintenance project size and sidewalk condition.

The remaining segments ranked from 635 to 824 are primarily in satisfactory or good condition and have no maintenance work identified.

3.1.4 Other Conditions

As part of the detailed inspections, the condition of several other sidewalk-related facilities was also collected.

The other conditions analyzed as part of the sidewalk assessment are as follows:

- ❑ Curb Condition – classified as a Good, Fair, or Poor condition level and reported separately.
- ❑ Paved Gutters – segments with pavement into the gutter were identified and used to adjust the curb conditions based on the amount of curb height remaining.
- ❑ Para-Ramp Condition – classified as a Good, Fair, Poor, or Missing condition.
- ❑ Obstructions – classified as having a Low, Medium, or High impact on pedestrian movement. They include vegetation overgrowth, utility encroachment, debris and other obstacles within the sidewalk R.O.W.

3.1.5 Maintenance Analysis

The maintenance analysis uses a defect-treatment approach to determine the quantity (slabs, length, or area) that require a specific application of maintenance or rehabilitation. The defects identified in the field survey are summarized by segment, and the maintenance criterion is applied to determine the treatments required for each affected location within the sidewalk segment. Additional criterion is applied to identify short-run (partial segment), and long-run (full segment), reconstruction requirements.

Based on discussion with the District the following policy was applied to the sidewalk and curb replacement recommendations. Sidewalks were selected for replacement based on their condition and/or where adjacent curb and gutter replacement is required. Likewise, curb segments were selected for replacement based on their performance and/or where adjacent sidewalk slabs require replacement.

MPE used the survey imagery to do a post survey review of the curb network to identify locations where pavement overlay encroached into the gutter. When the paved gutter caused the curb reveal to be too low (<5cm) the curbs were classified a Poor and identified for replacement.

Table 3.1 summarizes the sidewalk maintenance methodology used for the analysis.

Table 3.1: Sidewalk Maintenance Criteria

| APPLICATION LEVEL | MAINTENANCE CRITERIA | MAINTENANCE ACTIVITY |
|-------------------|--|----------------------|
| Slab | Defects: high and moderate severity corner and joint spalling, high severity small and large patches. | PCC Patching |
| Slab | Defects: high and moderate severity corner breaks and linear cracking. | PCC Crack Repair |
| Slab | Recommended to treat faulting of all severities | PCC Grinding |
| Slab | Recommended for high severity joint faulting when no other distresses are present in the slab | Mudjacking |
| Linear Meter | Recommended to treat linear cracking of medium to high severity | AC Crack Sealing |
| Square Meters | Recommended for high severity; cracking, distortions, and raveling | AC Patching |
| Square Meters | Recommended when the SCI of the segment is between 26 and 40 | AC Overlay |
| Square Meters | Recommended when the SCI of the segment is less than 26 | AC Reconstruction |
| Slab | Defects: high and moderate severity spalling, high severity divided slabs. | Slab Replacement |
| Full Segment | Recommended when more than 35% of the panels within a segment contain a high severity distress of any type, and the SCI of the segment is less than 60 | Slab Replacement |

The maintenance unit costs applied to the analysis are based on similar project experience and specific costs provided by the District. The unit rates are provided in **Table 3.2**.

Table 3.2: Maintenance and Replacement Costs

| MAINTENANCE ACTIVITY | UNIT COST | UNIT |
|---|------------|----------------|
| Concrete (PCC) Treatments | | |
| Crack Repair | \$17.25 | m |
| Patching | \$23.00 | m ² |
| PCC Grinding | \$25.30 | m ² |
| Mudjacking | \$138.00 | m ² |
| Slab Replacement (District Cost) | \$540.00 | m ² |
| Curb and Gutter Replacement (District Cost) | \$580.00 | m |
| Para Ramp Replacement (Single Letdown) | \$7,000.00 | Ramp |
| Asphalt (AC) Treatments | | |
| AC Crack Sealing | \$15.00 | m |
| AC Patching | \$10.00 | m ² |
| AC Overlay | \$50.00 | m ² |
| AC Curb Replacement (District Cost) | \$380.00 | m |
| AC Reconstruction | \$415.00 | m ² |

3.2 Network Results

The following section discusses and summarizes the condition of the District’s sidewalk network assessed in 2024. The **Sidewalk Condition and Capital/Maintenance Prioritized Listing**, summarized by network segment, is provided in **Appendix B**. The **Curb Condition and Capital/Maintenance Listing**, summarized by network segment, is provided in **Appendix C**.

The present status of the District’s sidewalk and curb networks are summarized in **Table 3.3**.

Table 3.3: Sidewalk and Curb Network Performance Summary

| FACILITY | SEGMENTS | LENGTH (KM) | #SLABS | SCI/Condition |
|------------------|----------|-------------|--------|---------------|
| Sidewalk Network | 816 | 110.7 | 31,968 | 76 |
| Curb Network | 1,121 | 162.3 | N/A | Fair |

The results of the other conditions analysis, including trip hazards, para-ramp deficiencies, and curb rail failures are summarized in **Table 3.4**.

Table 3.4: Sidewalk Critical Conditions Summary

| FACILITY | #FAILED SIDEWALKS | #TRIP HAZARDS (MOD/SEV) | #POOR -MISSING RAMPS | #SLAB REPLACEMENT |
|----------|-------------------|-------------------------|----------------------|-------------------|
| Sidewalk | 1 | 74 | 186 | 3,348 |

The results of the ramp conditions are summarized in **Table 3.5**.

Table 3.5: Sidewalk Ramp Summary

| CONDITION | #RAMPS |
|-------------|--------|
| Good | 498 |
| Fair | 60 |
| Poor/Failed | 22 |
| Missing | 164 |

A total of 580 ramps were inspected with the sidewalk survey. Locations where a ramp should be present, but is missing, are identified as missing.

Any missing and failed ramps are included in the full GIS inspection database provided to the District as part of the project delivery. The sidewalk segments with missing and/or failed para ramps are summarized in **Appendix D**.

The results indicate that concrete scaling and cracking are the most common high severity distress. In total 1,003 observations of high severity distresses/hazards were identified in the sidewalk survey. A summary showing the number of slabs identified at the high severity level for each distress type is provided in **Table 3.6**.

Table 3.6: High Severity Distress Summary

| DISTRESS | #SLABS |
|------------------------|----------------|
| Small Patch | 21 |
| Large Patch | 4 |
| Scaling | 472 |
| Shattering | 113 |
| Corner Break | 94 |
| Linear Cracking | 228 |
| Corner Spalling | 0 |
| Joint Spalling | 8 |
| Faulting | 19 |
| Trip Hazards | 44 |
| <i>Asphalt Defects</i> | <i>41 sq-m</i> |

The results of the pedestrian exposure analysis show the network distribution across the matrix classes and are summarized in **Table 3.7**.

Table 3.7: Pedestrian Exposure Summary

| CONDITION LEVEL | LENGTH (KM) | HAZARDS | | IMPORTANCE | | USAGE | |
|-----------------|-------------|---------|------|------------|------|--------|------|
| | | LENGTH | %LEN | LENGTH | %LEN | LENGTH | %LEN |
| Low | 110.7 | 104.7 | 93.8 | 0.0 | 0.0 | 0.0 | 0.0 |
| Medium | | 3.6 | 3.2 | 103.6 | 93.6 | 103.1 | 93.0 |
| High/Severe | | 3.3 | 3.0 | 7.1 | 6.4 | 7.7 | 7.0 |

3.2.1 Sidewalk Condition Summary

The results indicate most of the sidewalk network is in satisfactory condition with respect to the defects present.

Figure 3.2 shows the distribution of SCI values, weighted by centreline length.

The plot indicates a mean SCI of **76** for the sidewalk network.

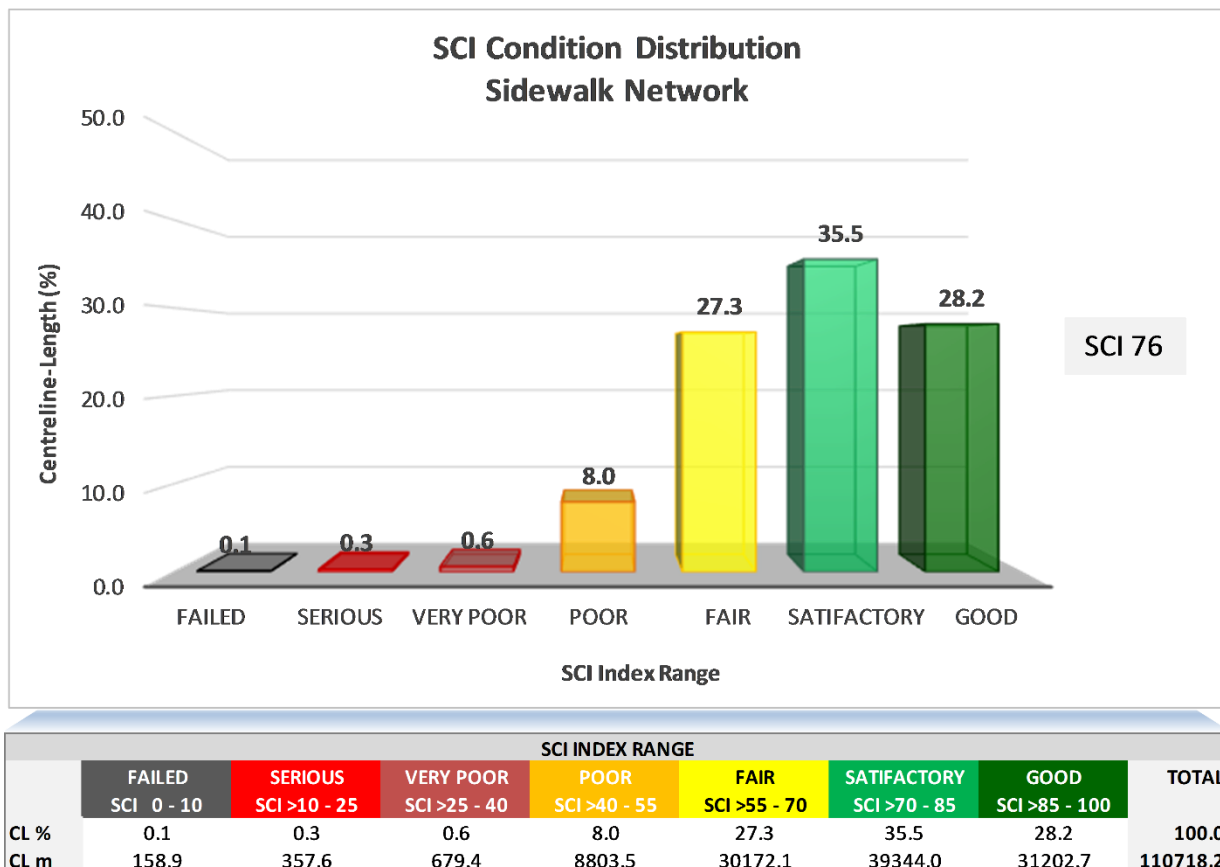


Figure 3.2: SCI Distribution – Sidewalk Network

Table 3.8 shows the statistical summary of the network SCI values.

Table 3.8: SCI Statistical Summary – Sidewalk Network

| STATISTIC | VALUE |
|-------------|-------|
| SCI Mean | 76 |
| SCI Minimum | 0.1 |
| SCI Maximum | 100.0 |
| SCI Std Dev | 15.0 |

3.2.2 Curb Condition Summary

The results indicate most of the curb network is in fair condition with respect to the defects present.

Figure 3.3 shows the condition distribution, weighted by centreline length.

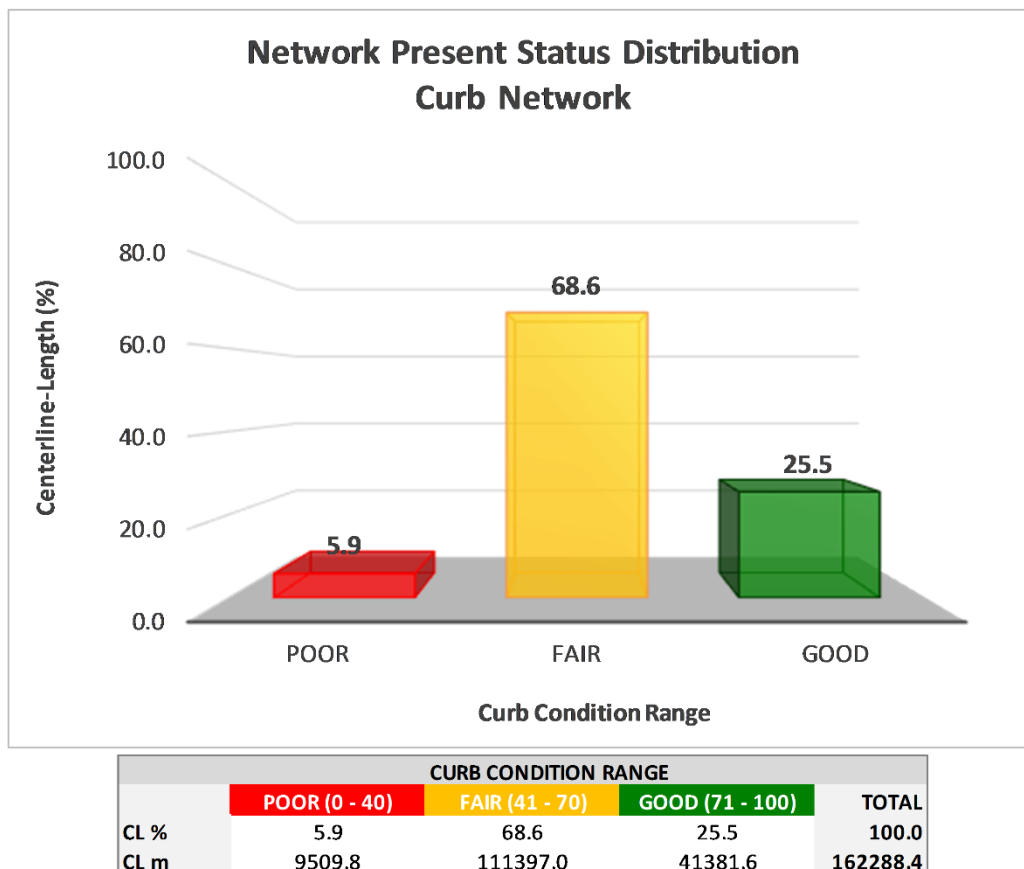


Figure 3.3: Condition Distribution – Curb Network

The review of the curb network identified 464 segments where the pavement overlays have encroached into the gutter. In the case of 53 segments the paved gutters caused the curb condition to be Poor.

3.2.3 Maintenance Summary

The results of the maintenance assessment show the overall length and cost of treatment, at each maintenance level.

Table 3.9 shows the current maintenance needs summary for the District’s sidewalk and curb networks assessments.

Table 3.9: Sidewalk and Curb Maintenance Level Summary

| TREATMENT | MAINTENANCE PROGRAM | TREATMENT QUANTITY | TREATMENT COST |
|------------------------------------|---------------------|--------------------|---------------------|
| PCC Crack Repair | Defect Repair | 5,672 m | \$97,859 |
| PCC Patching | | 297 sq-m | \$6,831 |
| PCC Grinding | | 255 sq-m | \$9,677 |
| Mudjacking | | 57 sq-m | \$11,799 |
| AC Crack Sealing | | 520 m | \$7,800 |
| AC Patch | | 31 sq-m | \$310 |
| Hazard Repair (Non-Defect related) | | 9 sq-m | \$400 |
| Slab Reconstruction | Replacement | 15,067 sq-m | \$8,135,640 |
| Para Ramp Replacement | Replacement | 837 sq-m | \$1,302,000 |
| AC Reconstruction | Replacement | 886 sq-m | \$367,690 |
| Sidewalk Total | | | \$9,940,006 |
| Curb Replacement | Replacement/Repair | 15,202 m | \$8,656,600 |
| TOTAL | | | \$18,596,606 |
| <i>Capital Projects</i> | | | <i>\$7,692,252</i> |
| <i>Maintenance Projects</i> | | | <i>\$10,904,354</i> |

The summary of the project cost requirements show that the overall costs are split between maintenance programs (57%) and capital projects (43%). The overall project costs are roughly equal between the sidewalk projects (53%) and the curb projects (47%).

The high capital cost for the slab replacement treatments is due to the Districts policy of replacing sidewalk slabs when the scaling defect has reached the moderate or severe category. Due to the age of the sidewalk network and the significant presence of moderate level scaling the analysis results are showing high cost requirements for slab replacement. The amount of slab replacement in the sidewalk network has also increased the amount and cost of the corresponding curb replacement require to occur simultaneously.

4.0 CONCLUSIONS AND RECOMMENDATIONS

4.1 Sidewalk Network

The following observations and conclusions are drawn from the sidewalk survey and analysis:

- ❑ The mean SCI of 76 indicates that overall, the sidewalk network is in satisfactory condition with respect to the defects present.
- ❑ A majority (64%) of the network is in satisfactory or good condition with SCI scores above 70.
- ❑ The results indicate that scaling is the most common recorded distress with 8,942 slabs affected throughout the network.
- ❑ The network included 14.9 km of asphalt sidewalks with an overall satisfactory condition.
- ❑ The critical conditions summary identified the following:
 - 74 trip hazards at the moderate or high severity level
 - 186 missing or failed para ramps
 - 1,003 observations of high severity distresses
- ❑ The maintenance results show that slab replacement is the most common recommended treatment.
- ❑ Budget analysis results indicate a total cost of \$9.9M is required in the sidewalk network. Capital projects will require \$3.2M (32%) and maintenance programs will require \$6.7M (68%). The high slab replacement costs are due the Districts policy of replacing slabs where the scaling distress has reached the moderate or severe level, and where adjacent curb replacements are required.
- ❑ There are 2,352 sidewalk slabs identified as having moderate-to-severe scaling, of which 472 (20%) are severe. The district may want to consider a 'lighter' treatment option (e.g. surface repair) for moderately scaled sidewalk slabs to reduce the replacement cost burden on the available budget.

4.2 Curb Network

The following observations and conclusions are drawn from the curb survey and analysis:

- ❑ The assessment of the curb network indicates an overall fair condition with respect to the defects present.
- ❑ A review of the distress results indicates that raveling and linear cracking are the most observed defects in the curb network. Most occurrences of these distresses are at the low severity level.
- ❑ The critical conditions summary identified the following:
 - 15,202 meters of curb will require replacement largely due to adjacent sidewalk replacement needs and paved gutters causing poor curb height conditions.
- ❑ Budget analysis results indicate a total cost of \$8.7M is required in the curb. Capital projects will require \$4.5M (52%) and maintenance programs will require \$4.2M (48%).

APPENDIX A

Concrete, Asphalt and Brick Paver
PCI Analysis Methodology

The ASTM Pavement Condition Index (PCI) is a measure of physical pavement cracking, deformations and surface defects collectively referred to as distresses. This provides an excellent indicator of material deficiency, rate of deterioration, structural adequacy, environmental and soil problems. The PCI is therefore a key indicator of the performance of roadways, airports, and parking lots. In addition, the PCI methodology is regularly applied to surfaced pedestrian thoroughfares.

PCI is used to monitor the condition of the networks, assess future needs, establish ranking and optimize expenditures. It will also provide information to monitor the performance of various design, rehabilitation, and maintenance techniques and to provide information for identifying candidate projects for maintenance and improvement programs.

The ASTM guidelines define distress identification, classification criteria, and deduct scoring methodologies for multiple surface material types, including flexible - asphalt (AC), rigid - concrete (PCC), and brick pavers. These different material types each have a unique set of deduct models (curves), but share a common PCI calculation methodology.

DISTRESS DEDUCT SCORES

To summarize the data for each segment, the distresses are combined into a single index (PCI), which is calculated using the deduct point system. The amount deducted is a function of the extent, type, and severity of the distress. Deduct Value (DV) models are set up for each distress type and are comprised of three curves of slight, moderate, and severe. The effective percent area quantity of the reported distress, at the identified severity level, is run through the DV model to determine the DV score of each distress type. The more critical the distress type (e.g. alligator cracking), the more severe the deduct modeling.

The DV type, distress density measurement, and the DV model coefficients 'a' and 'b' for the distresses included in PCI calculation, for the respective material type, are listed in **Tables A.1** through **A.3**.

Table A.1: Asphalt Distress Deduct Value Model Coefficients

| DISTRESS TYPE | QUANTITY MEASUREMENT | SLIGHT | | MODERATE | | SEVERE | |
|-----------------------|----------------------|--------|--------|----------|--------|--------|--------|
| | | Coef A | Coef B | Coef A | Coef B | Coef A | Coef B |
| Alligator Cr | % Area | 0.039 | 0.4136 | 0.284 | 0.3421 | 0.455 | 0.2839 |
| Map Cr | % Area | -1.052 | 0.8114 | -0.619 | 0.7034 | -0.209 | 0.5878 |
| Linear-Long. Cr | % Lineal/Area | -0.531 | 0.6419 | -0.075 | 0.4808 | 0.187 | 0.4997 |
| Linear-Trans. Cr | % Lineal/Area | -0.531 | 0.6419 | -0.075 | 0.4808 | 0.187 | 0.4997 |
| Edge Cr | % Lineal/Area | -0.536 | 0.5538 | -0.055 | 0.3960 | 0.171 | 0.3855 |
| Bleeding | % Area | -1.134 | 0.6962 | -0.563 | 0.6067 | -0.241 | 0.5655 |
| Distortion | % Area | -0.666 | 0.6533 | -0.076 | 0.5511 | 0.295 | 0.3930 |
| Rutting | % Area | -0.307 | 0.5507 | 0.117 | 0.4016 | 0.306 | 0.3711 |
| Rippling | % Area | -0.490 | 0.7179 | -0.007 | 0.5152 | 0.292 | 0.3844 |
| Raveling | % Area | -0.812 | 0.5202 | -0.065 | 0.3471 | 0.214 | 0.3670 |
| Patching/Utility Cuts | % Area | -0.871 | 0.4383 | -0.719 | 0.4878 | -0.338 | 0.4737 |
| Potholes | % Area | 0.664 | 0.5162 | 1.024 | 0.5780 | 1.102 | 0.3879 |

%Area - quantity is calculated as the percentage of *the Affected Area (sq. m)/Total Area (sq. m) of the segment.*

%Linear/Area - quantity is calculated as the percentage of *the Affected Linear Length (m)/Total Area (sq. m) of the segment.* For linear cracking the total length of cracking is divided by the segment area; for edge cracking the total affected edge length (both sides), is divided by the segment area.

When the asphalt PCI methodology is applied to paved trails and pathways, which have much narrower surfaces than typical roadways, modifications are made to the calculations of the %Linear/Area for the length-based quantities. This affects linear (L&T) and edge cracking. The measured quantities of these distresses are factored based on the facility width, to a standard two-lane eight m-wide roadway. The length is then used to calculate the ‘effective’ % Area used in the DV models. Original quantities are not affected in order to maintain accurate maintenance assessments.

In a trail/pathway context, the **%Linear/Area** is calculated as the percentage of *the [Affected Linear Length (m) x (Surface Width/8)] / Total Area (sq. m) of the segment.*

Table A.2: Concrete Distress Deduct Value Model Coefficients

| DISTRESS TYPE | QUANTITY MEASUREMENT | SLIGHT | | MODERATE | | SEVERE | |
|---------------------|-------------------------|---------------|---------------|---------------|---------------|--------------|---------------|
| | | Coef A | Coef B | Coef A | Coef B | Coef A | Coef B |
| Divided Slab | % Slabs-affected | -0.480 | 0.590 | 0.074 | 0.3958 | 0.214 | 0.3670 |
| Corner Break | % Slabs-affected | -0.721 | 0.7180 | -0.264 | 0.5381 | -0.241 | 0.5755 |
| Spalling, Corner | % Slabs-affected | -0.500 | 0.2800 | -0.240 | 0.2800 | -0.250 | 0.3600 |
| Spalling, Joint | % Slabs-affected | -0.500 | 0.2800 | -0.700 | 0.5600 | -0.531 | 0.6419 |
| Linear Cr | % Slabs-affected | -0.700 | 0.5600 | -0.500 | 0.5400 | -0.100 | 0.4500 |
| Faulting | % Slabs-affected | -0.718 | 0.6112 | -0.476 | 0.6098 | -0.241 | 0.5755 |
| Patching, Large | % Slabs-affected | -1.363 | 0.9318 | -0.721 | 0.7180 | -0.315 | 0.5971 |
| Patching, Small | % Slabs-affected | -2.500 | 1.0000 | -1.821 | 1.0000 | -0.700 | 0.5600 |
| Scaling | % Slabs-affected | -1.378 | 0.7484 | -0.592 | 0.5503 | -0.264 | 0.5381 |
| <i>Trip Hazards</i> | <i>% Slabs-affected</i> | <i>-0.509</i> | <i>0.5916</i> | <i>-0.241</i> | <i>0.5755</i> | <i>0.217</i> | <i>0.4357</i> |

%Slabs-affected - quantity is calculated as the percentage of *the Number of Slabs-affected/Total Number of Slabs in the segment*. In many sidewalk applications, the Slab length is standardized to 3 m.

Table A.3: Brick Paver Distress Deduct Value Model Coefficients

| DISTRESS TYPE | QUANTITY MEASUREMENT | SLIGHT | | MODERATE | | SEVERE | |
|---------------------|----------------------|---------------|---------------|---------------|---------------|--------------|---------------|
| | | Coef A | Coef B | Coef A | Coef B | Coef A | Coef B |
| Damaged Pavers | % Area | -0.650 | 0.3501 | 0.541 | 0.5850 | -0.241 | 0.5755 |
| Depressions | % Area | -0.065 | 0.3671 | -0.070 | 0.4231 | -0.075 | 0.4808 |
| Edge Restraint | % Area | -0.812 | 0.5202 | -0.536 | 0.5130 | -0.509 | 0.5916 |
| Faulting | % Area | -0.536 | 0.5030 | -0.055 | 0.3960 | 0.117 | 0.4016 |
| Heave | % Area | -0.509 | 0.5916 | -0.117 | 0.4016 | 0.306 | 0.3711 |
| Missing Pavers | % Area | -0.306 | 0.5550 | 0.400 | 0.8500 | 0.600 | 0.9990 |
| Patching | % Area | -0.981 | 0.4883 | -0.670 | 0.5114 | -0.509 | 0.5416 |
| <i>Trip Hazards</i> | <i>% Area</i> | <i>-0.509</i> | <i>0.5916</i> | <i>-0.241</i> | <i>0.5755</i> | <i>0.217</i> | <i>0.4357</i> |

INDIVIDUAL DISTRESS DEDUCT VALUES

The equation to calculate the individual distress DV is as follows:

$$DV_i = 10^{(a + b * \text{LOG}(\%Area))}$$

where

%Area = percent area of the distress/severity occurrence

The DV for a distress type is the sum of the combined severity-extend deduction for that distress type.

An example of a DV Curve set for the Divided Slab distress is graphically presented in **Figure A.1**.

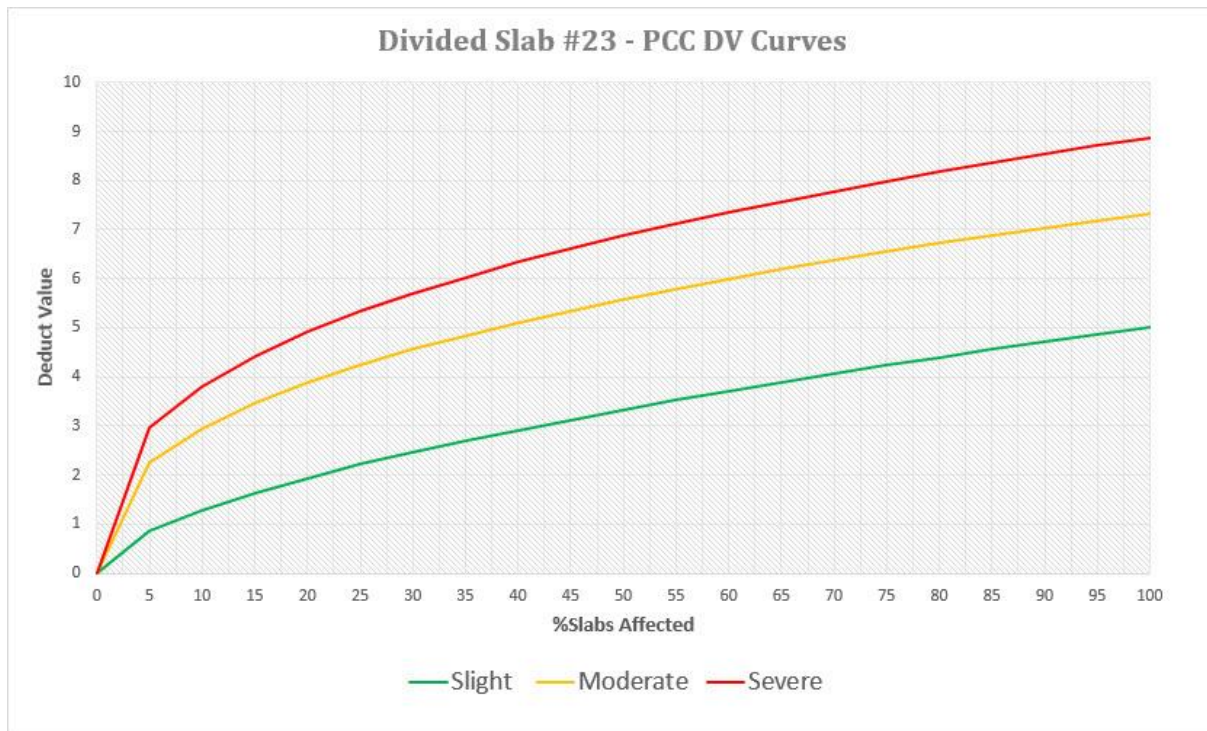


Figure A.1: DV Curve Set (Divided Slab)

ADJUSTED DISTRESS SCORES

The Total Deduct Value (TDV) is then calculated as the sum of the individual distress values in a given segment:

$$TDV = \text{sum}(DV_i)$$

The Adjusted Deduct Value (ADV) is then calculated from the TDV based on the Number of Equivalent Distresses (NED) present. The NED is calculated as the sum of the ratios of each distress value to the maximum distress value (DV_{max}). The DV_{max} is the largest DV observed for the data. This can be expressed as:

$$NED = \sum (DV_i / DV_{max})$$

where

DV_i = distress value for distress/severity level

DV_{max} = highest distress value observed

For roadway pavements, the ADV is calculated using the following equation:

$$ADV = 10 \times (10^{(-0.5 \times \text{LOG}(NED) + \text{LOG}(TDV))})$$

For sidewalks, trails and pathways, the ADV is calculated using the following equation:

$$ADV = 10 \times (10^{((0.1 - ((\text{LOG}(NED))^{NED})) \times \text{LOG}(NED)) + (1 - ((\text{LOG}(NED))^{(1 + (0.35 \times NED))})) \times \text{LOG}(TDV))})$$

The Distress Index (DI) used in the Pedestrian Exposure assessment is the ADV calculated on the 10-scale using the following equation:

$$DI = 10^{((0.1 - ((\text{LOG}(NED))^{NED})) \times \text{LOG}(NED)) + (1 - ((\text{LOG}(NED))^{(1 + (0.35 \times NED))})) \times \text{LOG}(TDV))}$$

The ADV–TDV correlation, for the roadway pavement application, is graphically presented in **Figure A.2**.

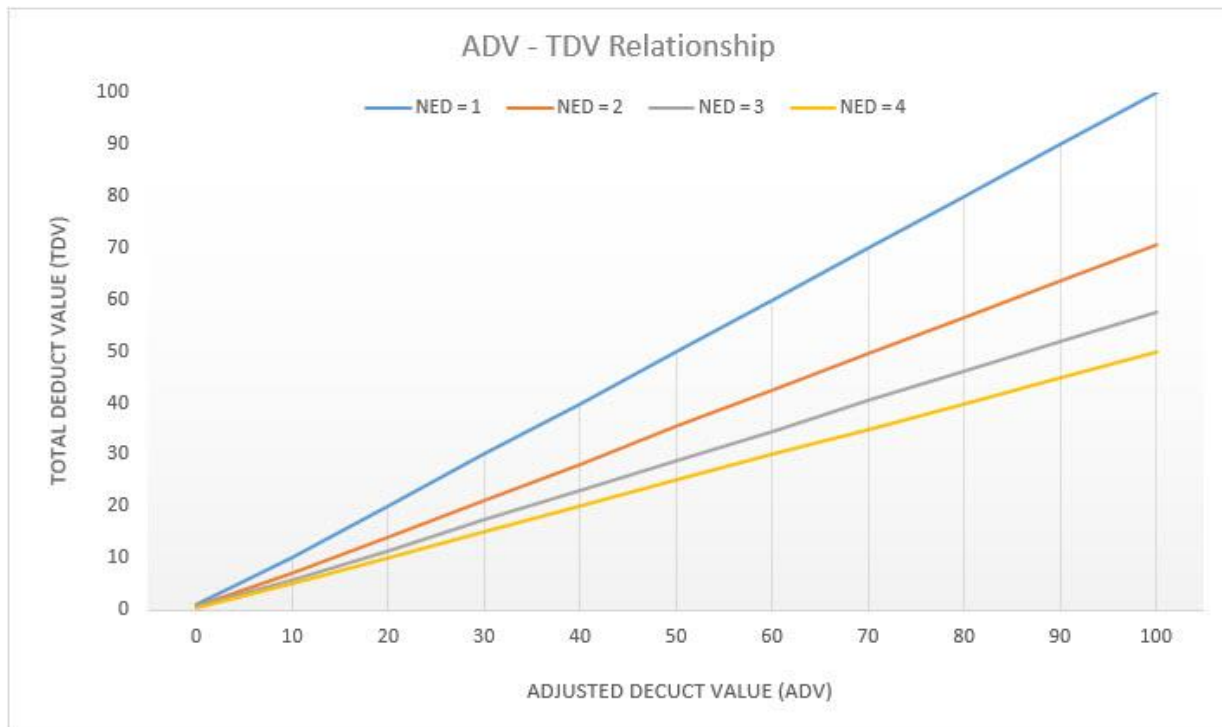


Figure A.2: ADV and TDV Correlation

PCI INDEX CALCULATION

Final PCI scores are calculated as follows:

$$PCI = PCI_M - ADV$$

where

PCI_M is the Maximum PCI score of 100

PCI values determined at 30-metre intervals were used to calculate segment equivalents. These segment values were then used to generate a summary distribution and mean for the network.

SIDEWALK PRIORITIZATION CALCULATIONS

The Distress Index (DI) is the ADV calculated on the 10-scale using the following equation:

$$DI = 10^{((0.1 - ((\text{LOG}(\text{NED}))^{\text{NED}}) \times \text{LOG}(\text{NED})) + (1 - ((\text{LOG}(\text{NED}))^{(1 + (0.35 \times \text{NED})))) \times \text{LOG}(\text{TDV}))}$$

The Pedestrian Index (PI) is determined from the Pedestrian Exposure matrix.

The Priority Value (PV) is calculated using the following equation:

$$PV = DI \times PI$$

APPENDIX B

Sidewalk Condition and Capital/Maintenance Prioritized Listing

DISTRICT OF OAK BAY
SIDEWALK NETWORK CONDITION SUMMARY
Sort: Priority Ranking

| ASST# | Condition | SW ID | Curb ID | Block | Street | From | To | SW | Data | Smb | SW | Total | Subs | SC | Priority | Failed-Reason | Failed-Missing | Pedestrian Exposure | Hazard | Usage | Total | Shots | Capital | PCC | PCC | Concrete Maintenance | | | | Asphalt Maintenance | | | | | | |
|--------------|-----------|-----------|----------|------------------|------------------------------|-----------------------|---------------------------------|-------|-------|------------|------------|-------|------|-----|----------|---------------|----------------|---------------------|----------|----------|-------|-------|---------|-------|-------|----------------------|----------|----------|----------|---------------------|----------|----------|----------|----------|-------|-------|
| | | | | | | | | | | | | | | | | | | | | | | | | | | Parade | Recon | Ramp | Hazard | AC | AC | AC | AC | Total | Recon | Recon |
| Foot | Very Poor | SOWK 85 | CRB 85 | S | Avondale Rd | Plymouth Rd | Cardiff Pl | 277.4 | PCC | Mntb | 2024-01-26 | 3.00 | 1.50 | 92 | 53 | 243 | 0 | Low | Medium | Moderate | 0 | 0 | 0 | 0 | \$155 | \$0 | \$0 | \$80,150 | \$0 | \$0 | \$0 | \$80,150 | 99 | 36% | | |
| Foot | Very Poor | SOWK 25 | CRB 25 | S | Alley St | Eastwode Rd | Eastwode Rd | 98.5 | PCC | Mntb | 2024-01-24 | 3.00 | 1.50 | 33 | 37 | 244 | 0 | Low | Medium | Moderate | 2 | Y | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$80,190 | \$0 | \$0 | \$0 | \$80,190 | 99 | 100% |
| Foot | Poor | SOWK 5218 | CRB 5218 | E | Central Ave | Central Ave | Central Ave | 211.8 | PCC | Mntb | 2024-01-26 | 3.00 | 1.50 | 24 | 24 | 246 | 0 | Low | High | Moderate | 8 | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$77,230 | \$0 | \$0 | \$0 | \$77,230 | 81 | 23% |
| Foot | Fair | SOWK 245 | CRB 245 | S | Beach Dr | Transit Rd | Inglwood Terr | 454.6 | PCC | Cr | 2024-01-15 | 3.00 | 1.50 | 152 | 66 | 246 | 0 | Low | Medium | Moderate | 2 | 0 | \$0 | \$0 | \$466 | \$114 | \$0 | \$68,040 | \$0 | \$0 | \$0 | \$68,040 | 84 | 18% | | |
| Foot | Fair | SOWK 137E | CRB 137E | E | Cardono Bay Rd | Cardono Rd | Cardono Rd | 393.3 | PCC | Mower | 2024-01-26 | 3.00 | 1.50 | 111 | 56 | 247 | 0 | Low | Medium | Moderate | 8 | 0 | \$0 | \$104 | \$114 | \$0 | \$68,040 | \$0 | \$0 | \$0 | \$68,040 | 84 | 25% | | | |
| Foot | Fair | SOWK 522E | CRB 522E | E | Cardono Bay Rd | Beach Dr | Cardono Bay Rd | 330.1 | PCC | Cr | 2024-01-24 | 3.00 | 1.50 | 123 | 62 | 249 | 0 | Low | Medium | Moderate | 4 | 4 | \$0 | \$0 | \$207 | \$0 | \$0 | \$0 | \$68,247 | \$0 | \$0 | \$0 | \$68,247 | 64 | 23% | |
| Foot | Fair | SOWK 551N | CRB 551N | N | Cardono Bay Rd | Plymouth Rd | Meridian Pl | 60.85 | PCC | Mntb | 2024-01-26 | 3.00 | 1.50 | 62 | 64 | 249 | 0 | Low | Medium | Moderate | 62 | 0 | \$0 | \$155 | \$0 | \$0 | \$0 | \$0 | \$60,905 | \$0 | \$0 | \$0 | \$60,905 | 75 | 40% | |
| Foot | Fair | SOWK 381S | CRB 381S | S | Lansdowne Rd | Lansdowne Rd | Eastwode Rd | 196.5 | PCC | Cr | 2024-01-24 | 3.00 | 1.50 | 66 | 64 | 250 | 0 | Low | Medium | Moderate | 26 | 0 | \$0 | \$0 | \$311 | \$0 | \$0 | \$53,460 | \$0 | \$0 | \$0 | \$53,771 | 66 | 34% | | |
| Foot | Fair | SOWK 112E | CRB 112E | E | Cardono Bay Rd | Neil St | Duffern Ave | 119.6 | PCC | Cr | 2024-01-25 | 3.00 | 1.50 | 40 | 58 | 251 | 0 | Low | Medium | Moderate | 37 | Y | \$0 | \$104 | \$0 | \$0 | \$0 | \$51,030 | \$0 | \$0 | \$0 | \$51,134 | 63 | 53% | | |
| Foot | Poor | SOWK 553E | CRB 553E | E | Cardono Bay Rd | Robertson Pl | Robertson Pl | 62.9 | PCC | Mntb | 2024-01-24 | 3.00 | 1.50 | 71 | 71 | 252 | 0 | Low | Medium | Moderate | 6 | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$51,030 | \$0 | \$0 | \$0 | \$51,030 | 63 | 100% | |
| Foot | Poor | SOWK 523E | CRB 523E | E | Oliver St | Central Ave | Timts St | 309.5 | PCC | Cr | 2024-01-16 | 3.00 | 1.50 | 103 | 52 | 253 | 0 | Low | Medium | Moderate | 1 | 0 | \$0 | \$466 | \$0 | \$0 | \$0 | \$46,370 | \$0 | \$0 | \$0 | \$46,636 | 57 | 18% | | |
| Foot | Poor | SOWK 552E | CRB 552E | E | Redwood Ave | Plymouth Rd | Rattaberry Pl | 129.9 | PCC | Mntb | 2024-01-26 | 3.00 | 1.50 | 43 | 40 | 254 | 0 | Low | Medium | Moderate | 1 | 0 | \$0 | \$52 | \$0 | \$0 | \$0 | \$46,370 | \$0 | \$0 | \$0 | \$46,422 | 57 | 44% | | |
| Foot | Poor | SOWK 597E | CRB 597E | E | Windor Rd | Windor Rd | Windor Rd | 233.8 | PCC | Cr | 2024-01-20 | 3.00 | 1.50 | 78 | 52 | 255 | 0 | Low | Medium | Moderate | 9 | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$41,740 | \$0 | \$0 | \$0 | \$41,740 | 54 | 23% | | |
| Foot | Poor | SOWK 103W | CRB 103W | W | Cardono Bay Rd | Dalhousie St | Tod Rd / Woodhouse Rd | 149.5 | PCC | Cr | 2024-01-23 | 3.00 | 1.50 | 50 | 65 | 256 | 0 | Low | High | High | 6 | 0 | \$0 | \$52 | \$114 | \$0 | \$0 | \$43,740 | \$0 | \$0 | \$0 | \$43,906 | 54 | 36% | | |
| Foot | Fair | SOWK 365N | CRB 365N | N | Kendal Ave | Plymouth Rd | Cardiff Pl | 173.4 | PCC | Mntb | 2024-01-26 | 3.00 | 1.50 | 58 | 64 | 257 | 0 | Low | Medium | Moderate | 3 | 0 | \$0 | \$104 | \$0 | \$0 | \$0 | \$43,844 | \$0 | \$0 | \$0 | \$43,844 | 54 | 31% | | |
| Foot | Poor | SOWK 15 | CRB 15 | S | Alley St | Foul Bay Rd | Henderson Rd | 215.6 | PCC | Mntb | 2024-01-24 | 3.00 | 1.50 | 72 | 52 | 258 | 0 | Low | Medium | Moderate | 1 | 0 | \$0 | \$776 | \$0 | \$0 | \$0 | \$42,300 | \$0 | \$0 | \$0 | \$42,300 | 51 | 24% | | |
| Foot | Fair | SOWK 114E | CRB 114E | E | Cardono Bay Rd | Lansdowne Rd | Lansdowne Rd | 290.8 | PCC | Mower | 2024-01-25 | 3.00 | 1.50 | 97 | 68 | 259 | 0 | Low | Medium | Moderate | 10 | 0 | \$0 | \$669 | \$0 | \$0 | \$0 | \$39,450 | \$0 | \$0 | \$0 | \$39,719 | 45 | 15% | | |
| Foot | Poor | SOWK 660E | CRB 660E | E | Victoria Ave | McNeil Ave | Sutherland Rd | 101.1 | PCC | Mntb | 2024-01-16 | 3.00 | 1.50 | 34 | 54 | 260 | 0 | Low | Medium | Moderate | 7 | 0 | \$0 | \$279 | \$0 | \$0 | \$0 | \$36,450 | \$0 | \$0 | \$0 | \$36,729 | 45 | 45% | | |
| Foot | Poor | SOWK 368S | CRB 368S | S | King George Terr | Crescent Rd | Barkeley Ter / King George Terr | 90.9 | PCC | Mntb | 2024-01-13 | 3.00 | 1.50 | 30 | 57 | 261 | 0 | Low | Medium | Moderate | 12 | Y | \$0 | \$52 | \$0 | \$0 | \$0 | \$36,450 | \$0 | \$0 | \$0 | \$36,502 | 45 | 50% | | |
| Foot | Fair | SOWK 1N | CRB 1N | N | Alley St | Foul Bay Rd | Foul Bay Rd | 212.2 | PCC | Mntb | 2024-01-24 | 3.00 | 1.50 | 71 | 62 | 262 | 0 | Low | Medium | Moderate | 5 | 0 | \$0 | \$414 | \$0 | \$0 | \$0 | \$34,020 | \$0 | \$0 | \$0 | \$34,334 | 42 | 20% | | |
| Satisfactory | SOWK 599W | CRB 599W | W | Stonehewer Pl | West End | Stonehewer Pl | 316.7 | PCC | Cr | 2024-01-26 | 3.00 | 1.50 | 106 | 74 | 263 | 0 | Low | Medium | Moderate | 4 | 0 | \$0 | \$311 | \$0 | \$0 | \$0 | \$0 | \$34,020 | \$0 | \$0 | \$0 | \$34,331 | 42 | 13% | | |
| Foot | Poor | SOWK 126E | CRB 126E | E | Cardiff Pl | Avondale Rd | Redwood Ave | 117.8 | PCC | Mntb | 2024-01-27 | 3.00 | 1.50 | 39 | 55 | 264 | 0 | Low | Medium | Moderate | 0 | 0 | \$0 | \$259 | \$0 | \$0 | \$0 | \$34,020 | \$0 | \$0 | \$0 | \$34,279 | 42 | 100% | | |
| Satisfactory | SOWK 375E | CRB 375E | E | King George Terr | Prince Andrew Pl | Prince Andrew Pl | 356.6 | PCC | Cr | 2024-01-13 | 3.00 | 1.50 | 119 | 74 | 265 | 0 | Low | Medium | Moderate | 1 | 0 | \$0 | \$155 | \$0 | \$0 | \$0 | \$0 | \$34,020 | \$0 | \$0 | \$0 | \$34,175 | 42 | 12% | | |
| Very Poor | SOWK 104E | CRB 104E | E | Cardono Bay Rd | Tod Rd / Woodhouse Rd | Hamiata St | 41.3 | PCC | Cr | 2024-01-23 | 3.00 | 1.50 | 14 | 41 | 266 | 0 | Low | Medium | Moderate | 5 | Y | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$34,020 | \$0 | \$0 | \$0 | \$34,020 | 42 | 100% | | |
| Satisfactory | SOWK 621S | CRB 621S | S | Townley St | Foul Bay Rd | Henderson Rd | 220.7 | PCC | Mntb | 2024-01-24 | 3.00 | 1.50 | 74 | 74 | 267 | 0 | Low | Medium | Moderate | 17 | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$34,020 | \$0 | \$0 | \$0 | \$34,020 | 42 | 19% | | | |
| Satisfactory | SOWK 589W | CRB 589W | W | St Patrick St | Brighton Ave | Oak Bay Ave | 235.4 | PCC | Cr | 2024-01-20 | 3.00 | 1.50 | 79 | 76 | 268 | 0 | Low | Medium | Moderate | 11 | 0 | \$0 | \$673 | \$0 | \$0 | \$0 | \$0 | \$33,590 | \$0 | \$0 | \$0 | \$33,268 | 49 | 17% | | |
| Satisfactory | SOWK 395E | CRB 395E | E | Lansdowne Rd | Lansdowne Rd | Lansdowne Rd | 273.0 | PCC | Mntb | 2024-01-25 | 3.00 | 1.50 | 260 | 260 | 269 | 0 | Low | Medium | Moderate | 6 | 0 | \$0 | \$466 | \$0 | \$0 | \$0 | \$0 | \$33,590 | \$0 | \$0 | \$0 | \$33,269 | 39 | 14% | | |
| Poor | SOWK 180S | CRB 180S | S | Dalhousie St | Mowat St | Dryfe St | 87.6 | PCC | Cr | 2024-01-23 | 3.00 | 1.50 | 29 | 53 | 270 | 0 | Low | Medium | Moderate | 3 | 0 | \$0 | \$307 | \$0 | \$0 | \$0 | \$0 | \$33,190 | \$0 | \$0 | \$0 | \$33,197 | 39 | 45% | | |
| Foot | Fair | SOWK 281E | CRB 281E | E | Foul Bay Rd | Townley St | Townley St | 82.8 | PCC | Cr | 2024-01-24 | 3.00 | 1.50 | 28 | 60 | 271 | 0 | Low | Medium | Moderate | 8 | 0 | \$0 | \$207 | \$0 | \$0 | \$0 | \$0 | \$33,190 | \$0 | \$0 | \$0 | \$33,797 | 39 | 47% | |
| Satisfactory | SOWK 500E | CRB 500E | E | King George Terr | Montney Creek Walkway | Montney Creek Walkway | 97.7 | PCC | Cr | 2024-01-24 | 3.00 | 1.50 | 27 | 32 | 272 | 0 | Low | Medium | Moderate | 6 | 0 | \$0 | \$104 | \$0 | \$0 | \$0 | \$0 | \$33,190 | \$0 | \$0 | \$0 | \$33,490 | 39 | 40% | | |
| Satisfactory | SOWK 473W | CRB 473W | W | Muzurave St | Cavendish Ave / Hampshire Rd | Dalhousie St | 91.9 | PCC | Mntb | 2024-01-23 | 3.00 | 1.50 | 31 | 80 | 273 | 0 | Low | Medium | Moderate | 5 | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$33,190 | \$0 | \$0 | \$0 | \$33,190 | 39 | 42% | | |
| Foot | Fair | SOWK 43E | CRB 43E | E | Estevan Ave | Estevan Ave | Thorge Pl | 200.5 | PCC | Mower | 2024-01-26 | 3.00 | 1.50 | 67 | 62 | 274 | 0 | Low | Medium | Moderate | 5 | 0 | \$104 | \$155 | \$0 | \$0 | \$0 | \$29,160 | \$0 | \$0 | \$0 | \$29,419 | 36 | 18% | | |
| Foot | Very Poor | SOWK 70N | CRB 70N | N | Estevan Ave | Estevan Ave | Estevan Ave | 80.6 | PCC | Cr | 2024-01-26 | 3.00 | 1.50 | 6 | 6 | 275 | 0 | Low | Medium | Moderate | 6 | 0 | \$0 | \$52 | \$0 | \$0 | \$0 | \$28,630 | \$0 | \$0 | \$0 | \$28,630 | 35 | 43% | | |
| Foot | Poor | SOWK 62N | CRB 62N | N | Bowker Ave | Eastwode Rd | Cardono Bay Rd | 222.8 | PCC | Cr | 2024-01-23 | 3.00 | 1.50 | 74 | 66 | 276 | 0 | Low | Medium | Moderate | 3 | 0 | \$0 | \$178 | \$0 | \$0 | \$0 | \$0 | \$26,730 | \$0 | \$0 | \$0 | \$27,117 | 33 | 15% | |
| Foot | Poor | SOWK 531S | CRB 531S | S | Pacific Ave | Eastwode Rd | Cardono Bay Rd | 172.7 | PCC | Mntb | 2024-01-24 | 3.00 | 1.50 | 58 | 56 | 277 | 0 | Low | Medium | Moderate | 4 | 0 | \$0 | \$706 | \$0 | \$0 | \$0 | \$0 | \$26,730 | \$0 | \$0 | \$0 | \$27,506 | 33 | 19% | |
| Foot | Fair | SOWK 39S | CRB 39S | S | Byron St | Foul Bay Rd | Foul Bay Rd | 234.7 | PCC | Mntb | 2024-01-23 | 3.00 | 1.50 | 79 | 69 | 278 | 0 | Low | Medium | Moderate | 9 | 0 | \$0 | \$569 | \$0 | \$0 | \$0 | \$0 | \$26,730 | \$0 | \$0 | \$0 | \$27,299 | 33 | 14% | |
| Satisfactory | SOWK 264E | CRB 264E | E | Foul Bay Rd | Brighton Ave | St. Patrick St | 343.2 | PCC | Mower | 2024-01-20 | 3.00 | 1.50 | 114 | 75 | 279 | 0 | Low | Medium | Moderate | 29 | 0 | \$104 | \$466 | \$0 | \$0 | \$0 | \$0 | \$26,730 | \$0 | \$0 | \$0 | \$27,299 | 33 | 10% | | |
| Fair | SOWK 597W | CRB 597W | W | St. George Terr | Windor Rd | Brighton Ave | 234.9 | PCC | Cr | 2024-01-20 | 3.00 | 1.50 | 78 | 67 | 280 | 0 | Low | Medium | Moderate | 48 | 0 | \$0 | \$466 | \$0 | \$0 | \$0 | \$0 | \$26,730 | \$0 | \$0 | \$0 | \$27,196 | 33 | 10% | | |
| Foot | Poor | SOWK 683W | CRB 683W | W | Woodburn Ave | South End | Wessex Cr | 216.9 | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

**DISTRICT OF OAK BAY
SIDEWALK NETWORK CONDITION SUMMARY**
Sort: Priority Ranking

| ACTM | SW ID | Curb ID | Block | Street | From | To | SW | Data | Slab | SW | Total | Priority | Failed- Missing | Pedestrian Exposure Hazard | Concrete Maintenance Usage | Total Slabs Affected | Capital Prop | PCC Paving | PCC C&G | PCC Concrete | Mid- Recon | Ramp Recon | Hazard Recon | AC Paving | AC C&G | Asphalt Maintenance | | Total Cost | Recon. Length (m) | Recon. Percent | | | |
|--------------|-------------|------------|-------|-------------------------|-------------------------|-----------------------------------|----|-------|------|-------|------------|----------|--------------------|-------------------------------|-------------------------------|----------------------------|-----------------|---------------|------------|-----------------|---------------|---------------|-----------------|--------------|-----------|---------------------|-----------|---------------|----------------------|-------------------|----------|-----|-----|
| | | | | | | | | | | | | | | | | | | | | | | | | | | AC Paving | AC C&G | | | | | | |
| Satisfactory | SDWK 465 | CRB 465 | S | Street | Cattle Point Lookout Rd | Cattle Point Lookout Rd | | 259.2 | PCC | Mowbr | 2024-01-26 | 3.00 | 1.50 | 86 | 78 | 365 | 0 | Low | Medium | Moderate | 0 | \$0 | \$362 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$10,082 | 12 | 5% |
| Satisfactory | SDWK 609E | CRB 609E | E | Westdowne Rd | Middlemore Rd | Landsdowne Rd | | 188.8 | PCC | Mowbr | 2024-01-24 | 3.00 | 1.50 | 63 | 73 | 365 | 0 | Low | Medium | Moderate | 0 | \$0 | \$311 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$10,031 | 12 | 6% |
| Satisfactory | SDWK 132E | CRB 132E | S | Eastdowne Rd | Eastdowne Rd | Eastdowne Rd | | 142.0 | PCC | Mowbr | 2024-01-26 | 3.00 | 1.50 | 73 | 76 | 365 | 0 | Low | Medium | Moderate | 2 | \$0 | \$311 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$10,031 | 12 | 6% |
| Satisfactory | SDWK 41E | CRB 41E | E | Beach Dr | Dalhousie St | Cavendish Ave | | 99.1 | PCC | Mowbr | 2024-01-22 | 3.00 | 1.50 | 33 | 73 | 367 | 0 | Low | Medium | Moderate | 0 | \$0 | \$155 | \$114 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,989 | 12 | 12% |
| Poor | SDWK 228E | CRB 228E | E | Elgin Rd | Chaucer St | Byron St | | 93.0 | PCC | Mntbl | 2024-01-22 | 3.00 | 1.50 | 31 | 51 | 368 | 0 | Low | Medium | Moderate | 2 | \$104 | \$155 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,979 | 12 | 13% | |
| Poor | SDWK 30E | CRB 30E | E | Elgin Rd | Orchard Ave | Curry St | | 94.1 | PCC | Mntbl | 2024-01-21 | 3.00 | 1.50 | 31 | 62 | 369 | 0 | Low | Medium | Moderate | 15 | \$0 | \$259 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,979 | 12 | 13% |
| Fair | SDWK 260W | CRB 260W | W | Beach Dr | Currie Rd | Goodwin St | | 147.1 | PCC | Mowbr | 2024-01-22 | 3.00 | 1.50 | 49 | 60 | 370 | 0 | Low | Medium | Moderate | 0 | \$0 | \$300 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,979 | 12 | 8% |
| Fair | SDWK 159S | CRB 159S | S | Cranmore Rd | Cardigan St | Christie Way | | 209.4 | PCC | Mntbl | 2024-01-23 | 3.00 | 1.50 | 70 | 62 | 371 | 0 | Low | High | High | 10 | \$0 | \$207 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,927 | 12 | 6% |
| Satisfactory | SDWK 214W | CRB 214W | W | Estowne Rd | Fair St | Hautain St | | 95.6 | PCC | Mntbl | 2024-01-23 | 3.00 | 1.50 | 32 | 76 | 372 | 0 | Low | Medium | Moderate | 6 | \$0 | \$207 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,927 | 12 | 13% |
| Satisfactory | SDWK 237S | CRB 237S | S | Estowne Rd | Cardigan St | Christie Way | | 209.4 | PCC | Mntbl | 2024-01-23 | 3.00 | 1.50 | 70 | 62 | 371 | 0 | Low | High | High | 10 | \$0 | \$207 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,927 | 12 | 4% |
| Fair | SDWK 499N | CRB 499N | N | Nottingham Rd | Thompson Ave | Duffern Ave | | 44.3 | PCC | Cr Gt | 2024-01-25 | 3.00 | 1.50 | 15 | 69 | 374 | 0 | Low | Medium | Moderate | 6 | \$0 | \$104 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,824 | 12 | 27% |
| Fair | SDWK 109E | CRB 109E | E | Cardigan St | Harlow Dr | Allenby St | | 96.4 | PCC | Cr Gt | 2024-01-25 | 3.00 | 1.50 | 32 | 69 | 374 | 0 | Low | Medium | Moderate | 0 | \$0 | \$104 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,824 | 12 | 12% |
| Fair | SDWK 327S | CRB 327S | S | Hautain St | Edward St | Thompson Ave | | 48.1 | PCC | Mntbl | 2024-01-23 | 3.00 | 1.50 | 16 | 67 | 376 | 0 | Low | Medium | Moderate | 2 | \$0 | \$52 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,772 | 12 | 25% |
| Satisfactory | SDWK 582E | CRB 582E | E | St Ann St | Montney Ave | Cranmore Rd | | 194.4 | PCC | Cr Gt | 2024-01-22 | 3.00 | 1.50 | 65 | 81 | 377 | 0 | Low | Medium | Moderate | 2 | \$0 | \$52 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,772 | 12 | 6% |
| Satisfactory | SDWK 101W | CRB 101W | W | Cardigan St | Bowker Ave | Bowker Ave | | 103.0 | PCC | Cr Gt | 2024-01-23 | 3.00 | 1.50 | 34 | 85 | 378 | 0 | Low | Medium | Moderate | 3 | \$0 | \$52 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,772 | 12 | 12% |
| Poor | SDWK 121E | CRB 121E | E | Cardigan St | Beach Dr | Hillbess St | | 20.8 | PCC | Cr Gt | 2024-01-26 | 3.00 | 1.50 | 7 | 95 | 379 | 0 | Low | Medium | Moderate | 9 | Y | \$0 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,720 | 12 | 58% |
| Fair | SDWK 134W | CRB 134W | W | Beach Dr | Fallside Rd | Mountroy Ave | | 155.8 | PCC | Mntbl | 2024-01-14 | 3.00 | 1.50 | 50 | 60 | 380 | 0 | Low | Medium | Moderate | 0 | \$0 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,720 | 12 | 8% | |
| Fair | SDWK 50E | CRB 50E | E | Beach Dr | Tarn Pl | Rutland Rd | | 78.8 | PCC | Mntbl | 2024-01-26 | 3.00 | 1.50 | 26 | 63 | 381 | 0 | Low | Medium | Moderate | 1 | \$0 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,720 | 12 | 15% | |
| Fair | SDWK 630S | CRB 630S | S | Tranin Rd | Newport Ave | Newport Ave | | 45.1 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 15 | 68 | 382 | 0 | Low | Medium | Moderate | 0 | \$0 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,720 | 12 | 27% | |
| Satisfactory | SDWK 110W | CRB 110W | W | Cardigan St | Allenby St | Thompson Ave | | 53.2 | PCC | Cr Gt | 2024-01-24 | 3.00 | 1.50 | 28 | 89 | 383 | 0 | Low | Medium | Moderate | 2 | \$0 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,720 | 12 | 23% | |
| Satisfactory | SDWK 656E | CRB 656E | E | Victoria Ave | Beaverbrooke St | Smythe St | | 89.4 | PCC | Cr Gt | 2024-01-16 | 3.00 | 1.50 | 30 | 78 | 384 | 0 | Low | Medium | Moderate | 0 | \$0 | \$0 | \$0 | \$9,720 | \$0 | \$0 | \$0 | \$0 | \$9,720 | 12 | 13% | |
| Fair | SDWK 364NS | CRB 364NS | NS | Kendal Ave | West End | Plymouth Rd | | 189.3 | PCC | Mntbl | 2024-01-27 | 3.00 | 1.50 | 63 | 69 | 385 | 0 | Low | Medium | Moderate | 2 | \$0 | \$414 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,704 | 9 | 5% |
| Fair | SDWK 288SW | CRB 288SW | SW | Frederick Norris Pl | Northwest End | Frederick Norris Rd / Redwood Ave | | 300.0 | PCC | Mntbl | 2024-01-27 | 3.00 | 1.50 | 100 | 70 | 386 | 0 | Low | Medium | Moderate | 17 | \$0 | \$414 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,704 | 9 | 5% |
| Satisfactory | SDWK 132EA | CRB 132EA | E | Cattle Point Lookout Rd | Beach Dr | Cattle Point Lookout Rd | | 231.7 | PCC | Cr Gt | 2024-01-26 | 3.00 | 1.50 | 77 | 77 | 387 | 0 | Low | Medium | Moderate | 5 | \$104 | \$311 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,704 | 9 | 4% | |
| Fair | SDWK 186E | CRB 186E | E | Deal St | Margrave Ave | Margrave Ave | | 142.0 | PCC | Mntbl | 2024-01-21 | 3.00 | 1.50 | 48 | 61 | 388 | 0 | Low | Medium | Moderate | 0 | \$0 | \$362 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,652 | 9 | 6% |
| Satisfactory | SDWK 300N | CRB 300N | N | Granite St | Victoria Ave | Victoria Ave | | 145.0 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 76 | 389 | 389 | 0 | Low | High | High | 2 | \$0 | \$504 | \$228 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,652 | 9 | 6% |
| Fair | SDWK 445E | CRB 445E | E | South Rd | Landsdowne Rd | Landsdowne Rd | | 104.0 | PCC | Mntbl | 2024-01-21 | 3.00 | 1.50 | 74 | 390 | 390 | 0 | Low | High | High | 21 | \$0 | \$311 | \$173 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,652 | 9 | 6% |
| Satisfactory | SDWK 663W | CRB 663W | W | Victoria Ave | Brighton Ave | Granite St | | 113.9 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 38 | 81 | 391 | 0 | Low | Medium | Moderate | 7 | \$311 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,601 | 9 | 8% | |
| Fair | SDWK 38E | CRB 38E | E | Beach Dr | Cranmore Rd | Ansonia Pl | | 106.7 | PCC | Mowbr | 2024-01-22 | 3.00 | 1.50 | 36 | 67 | 392 | 0 | Low | Medium | Moderate | 18 | \$0 | \$259 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,549 | 9 | 8% |
| Fair | SDWK 267E | CRB 267E | E | Beach Dr | Foul Bay Rd | Chaucer St | | 78.4 | PCC | Mntbl | 2024-01-22 | 3.00 | 1.50 | 37 | 64 | 393 | 0 | Low | Medium | Moderate | 0 | \$0 | \$259 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,549 | 9 | 13% |
| Satisfactory | SDWK 263E | CRB 263E | E | Foul Bay Rd | Quamanch St | Edgercliffe Pl | | 68.3 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 23 | 76 | 394 | 0 | Low | Medium | Moderate | 0 | \$207 | \$52 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,549 | 9 | 13% | |
| Fair | SDWK 205E | CRB 205E | E | Foul Bay Rd | Brighton Ave | Granite St | | 99.3 | PCC | Mowbr | 2024-01-20 | 3.00 | 1.50 | 33 | 56 | 395 | 0 | Low | Medium | Moderate | 2 | \$0 | \$207 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,549 | 9 | 9% |
| Fair | SDWK 671W | CRB 671W | W | North Rd | North Rd | Oak Bay Ave | | 215.3 | PCC | Mntbl | 2024-01-25 | 3.00 | 1.50 | 97 | 396 | 396 | 0 | Low | Medium | Moderate | 6 | \$0 | \$207 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,549 | 9 | 9% |
| Fair | SDWK 582W | CRB 582W | W | St Ann St | Cranmore Rd | Cranmore Rd | | 222.1 | PCC | Cr Gt | 2024-01-22 | 3.00 | 1.50 | 74 | 69 | 397 | 0 | Low | Medium | Moderate | 8 | \$0 | \$207 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,549 | 9 | 4% |
| Fair | SDWK 116E | CRB 116E | E | Cardigan St | Sherningham Pl | Cotswold Pl | | 127.4 | PCC | Mowbr | 2024-01-26 | 3.00 | 1.50 | 42 | 70 | 398 | 0 | Low | Medium | Moderate | 6 | \$0 | \$207 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,497 | 9 | 7% |
| Fair | SDWK 671E A | CRB 671E A | E | Winifred Pl | Oak Bay Ave | North End | | 44.4 | PCC | Cr Gt | 2024-01-21 | 3.00 | 1.50 | 14 | 61 | 399 | 0 | Low | Medium | Moderate | 6 | \$0 | \$155 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,445 | 9 | 22% |
| Fair | SDWK 132E | CRB 132E | E | Cardiff Pl | Windsor Rd | Windsor Rd | | 170.4 | PCC | Mntbl | 2024-01-27 | 3.00 | 1.50 | 40 | 60 | 400 | 0 | Low | Medium | Moderate | 0 | \$0 | \$155 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,445 | 9 | 22% |
| Satisfactory | SDWK 315E | CRB 315E | E | Hampshire Ter | Brighton Ave | Hampshire Ter | | 89.8 | PCC | Mntbl | 2024-01-20 | 3.00 | 1.50 | 30 | 75 | 401 | 0 | Low | Medium | Moderate | 15 | \$0 | \$155 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,445 | 9 | 10% |
| Satisfactory | SDWK 64S | CRB 64S | S | Bowker Ave | Hampshire Rd | St Ann St | | 92.7 | PCC | Mowbr | 2024-01-22 | 3.00 | 1.50 | 31 | 71 | 402 | 0 | Low | Medium | Moderate | 0 | \$0 | \$155 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,445 | 9 | 10% |
| Fair | SDWK 334W | CRB 334W | W | Henderson Rd | Henderson Rd | Towhies St | | 138.0 | PCC | Mntbl | 2024-01-27 | 3.00 | 1.50 | 43 | 60 | 403 | 0 | Low | High | High | 0 | \$0 | \$52 | \$0 | \$0 | \$7,290 | \$0 | \$0 | \$0 | \$0 | \$7,342 | 9 | 7% |
| Fair | SDWK 339W | CRB | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

DISTRICT OF OAK BAY
SIDEWALK NETWORK CONDITION SUMMARY
Sort: Priority Ranking

| ACTM Condition | SW ID | Curb ID | Block | Street | From | To | SW | Data | Slab | SW | Type | Status | Priority | Failed-Reason | Failed-Missing | Pedestrian Exposure | Total Slabs Affected | Capital Prop. | Concrete Maintenance | | | | Asphalt Maintenance | | | | | | | | | | | | | | | |
|----------------|-----------|----------|-------|---------------------|-------------------------|-------------------------------------|-------|------|-------|------------|------|--------|----------|---------------|----------------|---------------------|----------------------|---------------|----------------------|-----------|-----------|-------|---------------------|--------|-------------|---------|------------|-------|-----|-----|-----|-----|---------|---------|-------|-------|----|----|
| | | | | | | | | | | | | | | | | | | | PCC Patching | PCC Grout | Mid-Joint | Recon | Ramp | Hazard | AC Patching | AC Seal | AC Resurf. | Recon | | | | | | | | | | |
| Satisfactory | SOWK 470E | CRB 470E | E | Mowat St | Dalhousie St | Woodhouse Rd | 145.1 | PCC | Mntb | 2024-01-23 | 3.00 | 1.50 | 48 | 76 | 485 | 0 | Low | Medium | Moderate | 2 | \$0 | \$104 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 2% | | | |
| Satisfactory | SOWK 533E | CRB 533E | N | Pacific Ave | Top Ave | Thompson Ave | 118.5 | PCC | Mntb | 2024-01-25 | 3.00 | 1.50 | 40 | 78 | 486 | 0 | Low | Medium | Moderate | 0 | \$0 | \$104 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 3% | | |
| Satisfactory | SOWK 218E | CRB 218E | E | Cardioli Ave | Cardioli Ave | Cardioli Ave | 68.3 | PCC | Mntb | 2024-01-23 | 3.00 | 1.50 | 23 | 45 | 487 | 0 | Low | Medium | Moderate | 3 | \$0 | \$52 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 6% | | |
| Fair | SOWK 423E | CRB 423E | N | McNeil Ave | Victoria Ave | Victoria Ave | 36.9 | PCC | Mntb | 2024-01-16 | 3.00 | 1.50 | 12 | 65 | 488 | 0 | Low | Medium | Moderate | 2 | \$0 | \$52 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 8% | | |
| Satisfactory | SOWK 510S | CRB 510S | S | Oak Bay Ave | Wilmut Pl | Hampshire Rd | 59.7 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 20 | 76 | 489 | 0 | Low | Medium | Moderate | 0 | \$0 | \$52 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 1% | | |
| Satisfactory | SOWK 184N | CRB 184N | N | Cardioli Ave | Dalhousie St | Dalhousie St | 266.6 | PCC | Mntb | 2024-01-23 | 3.00 | 1.50 | 89 | 76 | 490 | 0 | Low | Medium | Moderate | 0 | \$0 | \$52 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 1% | | |
| Satisfactory | SOWK 637E | CRB 637E | E | Victoria Ave | Penland Rd | Penland Rd | 47.1 | PCC | Cr Gt | 2024-01-16 | 3.00 | 1.50 | 60 | 89 | 490 | 0 | Low | Medium | Moderate | 13 | \$0 | \$52 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 6% | | |
| Good | SOWK 596W | CRB 596W | W | St Patrick St | McNeil Ave | Wilmot Pl | 188.1 | PCC | Cr Gt | 2024-01-16 | 3.00 | 1.50 | 63 | 87 | 493 | 0 | Low | Medium | Moderate | 2 | \$0 | \$52 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 2% | | |
| Poor | SOWK 627W | CRB 627W | W | Tranier Rd | McNeil Ave | Wilmot Pl | 38.6 | PCC | Mntb | 2024-01-16 | 3.00 | 1.50 | 13 | 53 | 494 | 0 | Low | High | High | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 8% | | |
| Fair | SOWK 627E | CRB 627E | E | Tranier Rd | McNeil Ave | Wilmot Pl | 44.0 | PCC | Mntb | 2024-01-16 | 3.00 | 1.50 | 27 | 84 | 495 | 0 | Low | Medium | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 7% | | |
| Fair | SOWK 213W | CRB 213W | W | Eastdowne Rd | Dalhousie St | Fair St | 22.9 | PCC | Cr Gt | 2024-01-23 | 3.00 | 1.50 | 8 | 71 | 496 | 0 | Low | Medium | Moderate | 4 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 13% | | |
| Satisfactory | SOWK 85W | CRB 85W | W | Burkhead Rd | Mayhew St | Musgrave St | 32.7 | PCC | Mntb | 2024-01-25 | 3.00 | 1.50 | 11 | 72 | 497 | 0 | Low | Medium | Moderate | 2 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 9% | | |
| Satisfactory | SOWK 79S | CRB 79S | S | Monterey Ave | Oliver St | Monterey Ave | 80.2 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 27 | 74 | 498 | 0 | Low | Medium | Moderate | 2 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 4% | | |
| Satisfactory | SOWK 508S | CRB 508S | E | Oak Bay Ave | Elgin Rd | Yale St | 92.5 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 31 | 75 | 499 | 0 | Low | Medium | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 3% | | |
| Satisfactory | SOWK 348E | CRB 348E | E | Henderson Rd | Frederick Norris Rd | Cedar Hill Cross Rd / University Dr | 71.3 | PCC | Mntb | 2024-01-27 | 3.00 | 1.50 | 24 | 75 | 500 | 0 | Low | Medium | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 4% | | |
| Satisfactory | SOWK 681S | CRB 681S | S | Windsor Rd | St Denis St | Newport Ave | 13.4 | PCC | Cr Gt | 2024-01-21 | 3.00 | 1.50 | 4 | 85 | 501 | 0 | Low | High | High | 2 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 22% | | |
| Satisfactory | SOWK 462W | CRB 462W | W | Monterey Ave | McNeil Ave | Windsor Rd | 197.2 | PCC | Cr Gt | 2024-01-16 | 3.00 | 1.50 | 66 | 79 | 502 | 0 | Low | Medium | Moderate | 2 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 6% | | |
| Satisfactory | SOWK 45S | CRB 45S | S | Beach Dr | Dorset Rd | North Point Lookout Rd | 282.5 | PCC | Mower | 2024-01-26 | 3.00 | 1.50 | 94 | 81 | 503 | 0 | Low | Medium | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 1% | | |
| Satisfactory | SOWK 424N | CRB 424N | N | McNeil Ave | Victoria Ave | Rollin Rd | 44.9 | PCC | Mntb | 2024-01-16 | 3.00 | 1.50 | 15 | 81 | 504 | 0 | Low | Medium | Moderate | 5 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 7% | | |
| Satisfactory | SOWK 291S | CRB 291S | S | Frederick Norris Rd | Cardiff Pl | Henderson Rd | 80.0 | PCC | Mntb | 2024-01-27 | 3.00 | 1.50 | 27 | 84 | 505 | 0 | Low | Medium | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 4% | | |
| Satisfactory | SOWK 312E | CRB 312E | E | Hampshire Rd | McNeil Ave | Penland Rd | 98.5 | PCC | Cr Gt | 2024-01-16 | 3.00 | 1.50 | 33 | 84 | 506 | 0 | Low | Medium | Moderate | 5 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 3% | | |
| Satisfactory | SOWK 493E | CRB 493E | E | Newport Ave | Tranier Rd | Bereford St | 79.6 | PCC | Cr Gt | 2024-01-21 | 3.00 | 1.50 | 27 | 85 | 507 | 0 | Low | Medium | Moderate | 8 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 4% | | |
| Satisfactory | SOWK 210E | CRB 210E | E | Hampshire Rd | Oliver St | Beavenbrook St | 69.4 | PCC | Mntb | 2024-01-16 | 3.00 | 1.50 | 33 | 85 | 508 | 0 | Low | Medium | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 3% | | |
| Satisfactory | SOWK 621N | CRB 621N | N | Townley St | Foul Bay Rd | Henderson Rd | 218.6 | PCC | Cr Gt | 2024-01-24 | 3.00 | 1.50 | 73 | 86 | 509 | 0 | Low | Medium | Moderate | 2 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 1% | | |
| Good | SOWK 463W | CRB 463W | W | Monterey Ave | Windsor Rd | Brighton Ave | 244.7 | PCC | Cr Gt | 2024-01-20 | 3.00 | 1.50 | 78 | 91 | 510 | 0 | Low | Medium | Moderate | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 1% | | |
| Good | SOWK 161N | CRB 161N | N | Cromorne Rd | Armstrong Ave | Hampshire Rd | 36.3 | PCC | Mntb | 2024-01-23 | 3.00 | 1.50 | 15 | 92 | 511 | 0 | Low | Medium | Moderate | 2 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 6% | | |
| Good | SOWK 421N | CRB 421N | N | McNeil Ave | Armstrong Ave | Armstrong Ave | 148.3 | PCC | Mntb | 2024-01-16 | 3.00 | 1.50 | 15 | 92 | 512 | 0 | Low | Medium | Moderate | 10 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 6% | | |
| Good | SOWK 195W | CRB 195W | W | Dorset Rd | Nottingham Rd | Musgrave St | 267.6 | PCC | Mntb | 2024-01-25 | 3.00 | 1.50 | 89 | 97 | 513 | 0 | Low | Medium | Moderate | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,430 | 3 | 1% | | |
| Fair | SOWK 459W | CRB 459W | W | Monterey Ave | Lafayette St | Central Ave | 248.0 | PCC | Mntb | 2024-01-14 | 3.00 | 1.50 | 67 | 94 | 514 | 0 | Low | High | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,295 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,295 | 0 | 0% | | |
| Fair | SOWK 458W | CRB 458W | W | Monterey Ave | Lafayette St | Central Ave | 198.3 | PCC | Cr Gt | 2024-01-14 | 3.00 | 1.50 | 70 | 91 | 515 | 0 | Low | High | Moderate | 1 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,715 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$2,715 | 0 | 0% | | |
| Satisfactory | SOWK 343W | CRB 343W | W | Henderson Rd | Kendall Ave | Avondale Rd | 286.7 | PCC | Mntb | 2024-01-27 | 3.00 | 1.50 | 96 | 72 | 516 | 0 | Low | Medium | Moderate | 1 | \$0 | \$275 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$275 | 0 | 0% |
| Fair | SOWK 670E | CRB 670E | E | Westdowne Rd | Landowne Rd | Henderson Rd | 207.2 | PCC | Mntb | 2024-01-27 | 3.00 | 1.50 | 69 | 66 | 517 | 0 | Low | Medium | Moderate | 2 | \$0 | \$673 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$673 | 0 | 0% |
| Satisfactory | SOWK 628W | CRB 628W | W | Windsor Rd | Currie Rd | Windsor Rd | 143.7 | PCC | Mntb | 2024-01-23 | 3.00 | 1.50 | 42 | 83 | 518 | 0 | Low | Medium | Moderate | 1 | \$0 | \$104 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$104 | 0 | 0% |
| Satisfactory | SOWK 31E | CRB 31E | E | Beach Dr | Woodton St / Windsor Rd | Goodwin St | 218.5 | PCC | Mower | 2024-01-21 | 3.00 | 1.50 | 73 | 72 | 519 | 0 | Low | Medium | Moderate | 2 | \$104 | \$466 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$569 | 0 | 0% |
| Poor | SOWK 373S | CRB 373S | S | King George Ter | Woods St | King George Ter | 338.2 | AC | Mntb | 2024-01-25 | 3.00 | 1.50 | 41 | 520 | 0 | Low | Medium | Moderate | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$200 | \$300 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$500 | 0 | 0% | | |
| Satisfactory | SOWK 144S | CRB 144S | S | Central Ave | St Patrick St | Lyttel St | 85.0 | PCC | Cr Gt | 2024-01-14 | 3.00 | 1.50 | 82 | 521 | 0 | Low | Medium | Moderate | 0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$465 | 0 | 0% | |
| Fair | SOWK 268E | CRB 268E | E | Foul Bay Rd | Bygon St | Foul Bay Rd | 77.6 | PCC | Cr Gt | 2024-03-23 | 3.00 | 1.50 | 26 | 69 | 522 | 0 | Low | Medium | Moderate | 2 | \$0 | \$414 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$0 | \$414 | 0 | 0% |
| Satisfactory | SOWK 521E | CRB 521E | E | Oakdowne Rd | Middlowe Rd | Eastdowne Rd | 264.2 | AC | Mntb | 2024-01-24 | 3.00 | 1.50 | 83 | 523 | | | | | | | | | | | | | | | | | | | | | | | | |

APPENDIX C

Curb Condition and Capital/Maintenance Listing

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | |
|----------------|-----------|-----------|-------|---------------|---------------------------|-------------------------|--------|----------|--------|-------|-----|---------|------------------|----------|-----------|----------------|-------------------|----------------|---------------|
| | | | Face | Street | | | | | | | | | Gutter | Priority | Proj? | PCC Length (m) | | | AC Length (m) |
| Fair | CRB_1N | SDWK_1N | N | Allenby St | Foul Bay Rd | Henderson Rd | 216.4 | PCC | Mntbl | N | 262 | N | 42.0 | 0.0 | \$24,360 | \$0 | \$24,360 | 42 | 19% |
| Fair | CRB_1S | SDWK_1S | S | Allenby St | Foul Bay Rd | Henderson Rd | 216.4 | PCC | Mntbl | N | 258 | N | 51.0 | 0.0 | \$29,580 | \$0 | \$29,580 | 51 | 24% |
| Fair | CRB_2N | SDWK_2N | N | Allenby St | Henderson Rd | Eastdowne Rd | 258.6 | PCC | Mntbl | N | 63 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 12% |
| Fair | CRB_2S_A | SDWK_2S_A | S | Allenby St | Henderson Rd | Eastdowne Rd | 98.5 | PCC | Mntbl | N | 244 | Y | 99.0 | 0.0 | \$57,420 | \$0 | \$57,420 | 99 | 100% |
| Good | CRB_2S_B | SDWK_2S_B | S | Allenby St | Henderson Rd | Eastdowne Rd | 155.6 | PCC | Cr_Gtt | N | 347 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 10% |
| Fair | CRB_3N | SDWK_3N | N | Allenby St | Eastdowne Rd | Cadboro Bay Rd | 75.9 | PCC | Mntbl | N | 615 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_3N_A | | N | Allenby St | Eastdowne Rd | Cadboro Bay Rd | 95.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_3S | SDWK_3S | S | Allenby St | Eastdowne Rd | Cadboro Bay Rd | 78.9 | PCC | Mntbl | N | 625 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_3S_A | | S | Allenby St | Eastdowne Rd | Cadboro Bay Rd | 96.8 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_4N | | N | Anscomb Pl | Northwest End | Beach Dr | 99.2 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 6% |
| Good | CRB_4S | | S | Anscomb Pl | Northwest End | Beach Dr | 101.4 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 6% |
| Fair | CRB_5N | SDWK_5N | N | Arden Rd | Henderson Rd | East End | 77.0 | PCC | Mntbl | N | 558 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_5S | SDWK_5S | S | Arden Rd | Henderson Rd | East End | 73.9 | PCC | Mntbl | N | 542 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_6WE | | WE | Armstrong Ave | Southwest End | Cranmore Rd | 295.3 | PCC | Mntbl | Y | 0 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 3% |
| Fair | CRB_7E | SDWK_7E | E | Ashdowne Rd | Sandowne Rd | Middowne Rd | 115.9 | PCC | Mntbl | N | 191 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_7W | SDWK_7W | W | Ashdowne Rd | Sandowne Rd | Middowne Rd | 106.2 | PCC | Mntbl | N | 77 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_10N | SDWK_10N | N | Avondale Rd | Henderson Rd | East End | 49.8 | PCC | Mntbl | N | 538 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_10S | SDWK_10S | S | Avondale Rd | Henderson Rd | East End | 46.5 | PCC | Mntbl | N | 672 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_8N | SDWK_8N | N | Avondale Rd | Plymouth Rd | Cardiff Pl | 288.4 | PCC | Mntbl | N | 136 | N | 66.0 | 0.0 | \$38,280 | \$0 | \$38,280 | 66 | 23% |
| Fair | CRB_8S | SDWK_8S | S | Avondale Rd | Plymouth Rd | Cardiff Pl | 285.8 | PCC | Mntbl | N | 243 | N | 99.0 | 0.0 | \$57,420 | \$0 | \$57,420 | 99 | 35% |
| Fair | CRB_9N | SDWK_9N | N | Avondale Rd | Cardiff Pl | Henderson Rd | 98.5 | PCC | Mntbl | N | 408 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 9% |
| Fair | CRB_9S | SDWK_9S | S | Avondale Rd | Cardiff Pl | Henderson Rd | 97.1 | PCC | Mntbl | N | 171 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_11W | | W | Barkley Ter | King George Terr | Northwest End | 389.1 | AC | Mntbl | N | 0 | N | 0.0 | 2.0 | \$760 | \$0 | \$760 | 2 | 1% |
| Fair | CRB_12N | | N | Bartlett Ave | Victoria Ave | Hampshire Rd | 126.5 | AC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_12S | | S | Bartlett Ave | Victoria Ave | Hampshire Rd | 127.4 | AC | Mntbl | Y | 0 | N | 0.0 | 1.0 | \$380 | \$0 | \$380 | 1 | 1% |
| Fair | CRB_13E | SDWK_13E | E | Beach Dr | Denison Rd / Fairfield Rd | Mountjoy Ave | 107.4 | PCC | Mntbl | Y | 213 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_13W | SDWK_13W | W | Beach Dr | Denison Rd / Fairfield Rd | Mountjoy Ave | 94.2 | PCC | Mntbl | Y | 576 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_14E | | E | Beach Dr | Mountjoy Ave | Falkland Rd | 172.3 | PCC | Mntbl | Y | 0 | Y | 171.0 | 0.0 | \$99,180 | \$0 | \$99,180 | 171 | 99% |
| Fair | CRB_14W | SDWK_14W | W | Beach Dr | Mountjoy Ave | Falkland Rd | 156.1 | PCC | Mntbl | Y | 380 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 8% |
| Fair | CRB_15E | | E | Beach Dr | Falkland Rd | Sylvan Lane | 85.7 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_15W | SDWK_15W | W | Beach Dr | Falkland Rd | Sylvan Lane | 50.7 | PCC | Mntbl | N | 768 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_17E | | E | Beach Dr | Sylvan Ln | Victoria Ave | 106.6 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_17W | SDWK_17W | W | Beach Dr | Sylvan Ln | Victoria Ave | 92.4 | PCC | Mntbl | Y | 769 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_18E | | E | Beach Dr | Victoria Ave | Sunset Ave | 67.5 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Poor | CRB_18W | SDWK_18W | W | Beach Dr | Victoria Ave | Sunset Ave | 72.3 | PCC | Mntbl | Y | 228 | Y | 72.0 | 0.0 | \$41,760 | \$0 | \$41,760 | 72 | 100% |
| Fair | CRB_19E | | E | Beach Dr | Sunset Ave | King George Ter | 93.1 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_19W | SDWK_19W | W | Beach Dr | Sunset Ave | King George Ter | 83.5 | PCC | Mntbl | N | 600 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_20N | | N | Beach Dr | King George Ter | Monterey Ave | 164.9 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_20S | SDWK_20S | S | Beach Dr | King George Ter | Monterey Ave | 173.6 | PCC | Cr_Gtt | N | 426 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_21N | | N | Beach Dr | Monterey Ave | Oliver St | 134.8 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Poor | CRB_21S | SDWK_21S | S | Beach Dr | Monterey Ave | Oliver St | 146.8 | PCC | Cr_Gtt | Y | 218 | Y | 147.0 | 0.0 | \$85,260 | \$0 | \$85,260 | 147 | 100% |
| Fair | CRB_22N | | N | Beach Dr | Oliver St | St Patrick St | 89.4 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_22S | SDWK_22S | S | Beach Dr | Oliver St | St Patrick St | 99.4 | PCC | Cr_Gtt | Y | 738 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_23N | | N | Beach Dr | St Patrick St | Transit Rd | 146.2 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_23S | SDWK_23S | S | Beach Dr | St Patrick St | Transit Rd | 163.2 | PCC | Mntbl | Y | 643 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_24S | SDWK_24S | S | Beach Dr | Transit Rd | Inglewood Ter | 458.0 | PCC | Cr_Gtt | Y | 246 | N | 84.0 | 0.0 | \$48,720 | \$0 | \$48,720 | 84 | 18% |
| Poor | CRB_26S | SDWK_26S | S | Beach Dr | Hood Ln | Newport Ave | 205.2 | PCC | Mower | Y | 217 | Y | 204.0 | 0.0 | \$118,320 | \$0 | \$118,320 | 204 | 99% |
| Fair | CRB_27E | SDWK_27E | E | Beach Dr | Newport Ave | Margate Ave | 1243.0 | PCC | Mntbl | N | 237 | N | 171.0 | 0.0 | \$99,180 | \$0 | \$99,180 | 171 | 14% |
| Good | CRB_27W | SDWK_27W | W | Beach Dr | Newport Ave | Margate Ave | 221.5 | PCC | Mntbl | N | 28 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 9% |
| Fair | CRB_27W_A | | W | Beach Dr | Newport Ave | Margate Ave | 990.1 | PCC | Mntbl | N | 0 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 1% |
| Fair | CRB_28E | SDWK_28E | E | Beach Dr | Margate Ave | Satellite St | 24.4 | PCC | Mntbl | N | 711 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_28W | | W | Beach Dr | Margate Ave | Satellite St | 28.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_29E | SDWK_29E | E | Beach Dr | Satellite St | Orchard Ave | 293.7 | PCC | Mntbl | N | 118 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 6% |
| Poor | CRB_29W | SDWK_29W | W | Beach Dr | Satellite St | Orchard Ave | 286.0 | PCC | Mntbl | N | 787 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_30E | SDWK_30E | E | Beach Dr | Orchard Ave | Currie Rd | 94.7 | PCC | Mntbl | N | 369 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 13% |
| Fair | CRB_30W | | W | Beach Dr | Orchard Ave | Currie Rd | 84.2 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_31E | SDWK_31E | E | Beach Dr | Currie Rd | Goodwin St / Windsor Rd | 186.1 | PCC | Mower | Y | 519 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_31E | SDWK_31E | E | Beach Dr | Currie Rd | Goodwin St / Windsor Rd | 209.3 | PCC | Mower | Y | 519 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_32E | SDWK_32E | E | Beach Dr | Goodwin St / Windsor Rd | Beresford St | 251.2 | PCC | Mower | N | 760 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_32W | | W | Beach Dr | Goodwin St / Windsor Rd | Beresford St | 238.7 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_33E | SDWK_33E | E | Beach Dr | Beresford St | Broom Rd | 216.3 | PCC | Mntbl | N | 461 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_33W | | W | Beach Dr | Beresford St | Broom Rd | 202.8 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_34E | SDWK_34E | E | Beach Dr | Broom Rd | San Carlos Ave | 102.6 | PCC | Cr_Gtt | N | 704 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_34W | SDWK_34W | W | Beach Dr | Broom Rd | San Carlos Ave | 92.6 | PCC | Mntbl | N | 721 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_35E | SDWK_35E | E | Beach Dr | Shady Ln | San Carlos Ave | 80.4 | PCC | Cr_Gtt | N | 436 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% |
| Fair | CRB_35W | | W | Beach Dr | Shady Ln | Shady Ln | 75.8 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_36E | SDWK_36E | E | Beach Dr | Shady Ln | Somass Dr | 65.0 | PCC | Mower | N | 198 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 14% |
| Fair | CRB_36W | | W | Beach Dr | Shady Ln | Somass Dr | 66.3 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_37E | SDWK_37E | E | Beach Dr | Somass Dr | Cranmore Rd | 19.8 | PCC | Cr_Gtt | N | 795 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_37W | | W | Beach Dr | Somass Dr | Cranmore Rd | 17.3 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_38E | SDWK_38E | E | Beach Dr | Cranmore Rd | Anscomb Pl | 107.5 | PCC | Mower | N | 392 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 8% |
| Fair | CRB_38W | | W | Beach Dr | Cranmore Rd | Anscomb Pl | 95.1 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_39E | SDWK_39E | E | Beach Dr | Anscomb Pl | Bowker Ave | 103.7 | PCC | Mower | N | 593 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_39W | | W | Beach Dr | Anscomb Pl | Bowker Ave | 95.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_40E | SDWK_40E | E | Beach Dr | Bowker Ave | Cavendish Ave | 101.8 | PCC | Mower | Y | 297 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 27% |
| Fair | CRB_40W | | W | Beach Dr | Bowker Ave | Cavendish Ave | 98.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |

**DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY**

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | From | To | Length | Material | Type | Paved | | | Curb Replacement | | AC Cost | Total Cost | Recon. | | |
|----------------|----------|-----------|-------|------------------------|------|-------------------------|--------|----------|--------|--------|----------|-------|------------------|---------------|----------|------------|----------|------------|---------|
| | | | Face | Street | | | | | | Gutter | Priority | Proj? | PCC Length (m) | AC Length (m) | | | PCC Cost | Length (m) | Percent |
| Fair | CRB_41E | SDWK_41E | E | Beach Dr | | Dalhousie St | 104.1 | PCC | Mower | Y | 367 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 12% |
| Fair | CRB_41W | SDWK_41W | W | Beach Dr | | Cavendish Ave | 98.8 | PCC | Mntbl | Y | 732 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_42E | SDWK_42E | E | Beach Dr | | Dalhousie St | 285.2 | PCC | Mower | N | 35 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 3% |
| Fair | CRB_42W | | W | Beach Dr | | Dalhousie St | 287.3 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$0 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_43E | SDWK_43E | E | Beach Dr | | Estevan Ave | 206.4 | PCC | Mower | N | 274 | N | 36.0 | 0.0 | \$20,880 | \$0 | \$20,880 | 36 | 17% |
| Good | CRB_43W | | W | Beach Dr | | Estevan Ave | 203.3 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_44E | SDWK_44E | E | Beach Dr | | Thorpe Pl | 167.9 | PCC | Mower | N | 638 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_44W | | W | Beach Dr | | Thorpe Pl | 156.9 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_45N | | N | Beach Dr | | Dorset Rd | 297.6 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% |
| Fair | CRB_45S | SDWK_45S | S | Beach Dr | | Dorset Rd | 287.4 | PCC | Mower | N | 503 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_46N | | N | Beach Dr | | Cattle Point Lookout Rd | 253.7 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_46S | SDWK_46S | S | Beach Dr | | Cattle Point Lookout Rd | 263.0 | PCC | Mower | N | 364 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 5% |
| Fair | CRB_47E | SDWK_47E | E | Beach Dr | | Cattle Point Lookout Rd | 207.6 | PCC | Mower | N | 806 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_47W | | W | Beach Dr | | Cattle Point Lookout Rd | 210.0 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_48E | SDWK_48E | E | Beach Dr | | Surrey Rd | 123.7 | PCC | Mntbl | Y | 617 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_48W | | W | Beach Dr | | Surrey Rd | 133.0 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_49E | SDWK_49E | E | Beach Dr | | Valdez Pl | 37.6 | PCC | Mntbl | N | 178 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_49W | | W | Beach Dr | | Valdez Pl | 40.5 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_50E | SDWK_50E | E | Beach Dr | | Tarn Pl | 78.2 | PCC | Mntbl | N | 381 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 15% |
| Fair | CRB_50W | | W | Beach Dr | | Tarn Pl | 76.5 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_51E | SDWK_51E | E | Beach Dr | | Rutland Rd | 315.7 | PCC | Mower | Y | 65 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_51W | | W | Beach Dr | | Rutland Rd | 303.5 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_52E | SDWK_52E | E | Beach Dr | | Lansdowne Rd | 280.4 | PCC | Mower | N | 97 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_52W | | W | Beach Dr | | Lansdowne Rd | 276.4 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_53E | SDWK_53E | E | Beach Dr | | Humber Rd | 168.3 | PCC | Mower | N | 532 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_53W | | W | Beach Dr | | Humber Rd | 154.8 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_54E | SDWK_54E | E | Beach Dr | | Exeter Rd | 192.3 | PCC | Mower | N | 807 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_54W | | W | Beach Dr | | Exeter Rd | 190.3 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_55E | SDWK_55E | E | Beach Dr | | Ripon Rd | 191.3 | PCC | Mower | N | 567 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_55W | | W | Beach Dr | | Ripon Rd | 187.6 | PCC | Cr_Gtt | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_56E | SDWK_56E | E | Beach Dr | | Beach Dr to Midland Rd | 161.8 | PCC | Mower | Y | 692 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_56W | | W | Beach Dr | | Beach Dr to Midland Rd | 129.4 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_57E | SDWK_57E | E | Beach Dr | | Midland Rd | 324.1 | PCC | Mower | Y | 419 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% |
| Fair | CRB_57W | | W | Beach Dr | | Midland Rd | 296.2 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_58N | | N | Beach Dr to Midland Rd | | Midland Rd | 79.6 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_58S | SDWK_58S | S | Beach Dr to Midland Rd | | Midland Rd | 98.5 | PCC | Mower | Y | 467 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_59N | SDWK_59N | N | Beaverbrooke St | | Victoria Ave | 127.7 | PCC | Cr_Gtt | N | 442 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% |
| Good | CRB_59S | | S | Beaverbrooke St | | Victoria Ave | 124.2 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_60E | SDWK_60E | E | Bee St | | Goldsmith St | 217.4 | PCC | Cr_Gtt | N | 27 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Good | CRB_60W | SDWK_60W | W | Bee St | | Goldsmith St | 206.6 | PCC | Cr_Gtt | N | 335 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 9% |
| Fair | CRB_61S | | S | Beresford St | | Newport Ave | 37.7 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_62N | SDWK_62N | N | Bowker Ave | | Eastdowne Rd | 227.5 | PCC | Cr_Gtt | Y | 276 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 15% |
| Fair | CRB_62S | SDWK_62S | S | Bowker Ave | | Eastdowne Rd | 224.0 | PCC | Cr_Gtt | Y | 321 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 9% |
| Good | CRB_63N | SDWK_63N | N | Bowker Ave | | Cadboro Bay Rd | 90.1 | PCC | Cr_Gtt | Y | 588 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_63S | SDWK_63S | S | Bowker Ave | | Cadboro Bay Rd | 88.2 | PCC | Mntbl | Y | 359 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 17% |
| Fair | CRB_64N | SDWK_64N | N | Bowker Ave | | Hampshire Rd | 94.3 | PCC | Mntbl | Y | 435 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 6% |
| Fair | CRB_64S | SDWK_64S | S | Bowker Ave | | Hampshire Rd | 93.2 | PCC | Mower | Y | 402 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 10% |
| Fair | CRB_65N | SDWK_65N | N | Bowker Ave | | St Ann St | 84.9 | PCC | Mntbl | Y | 437 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% |
| Fair | CRB_65S | SDWK_65S | S | Bowker Ave | | St Ann St | 74.6 | PCC | Mntbl | Y | 92 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 12% |
| Good | CRB_66N | SDWK_66N | N | Bowker Ave | | Lulie St | 90.4 | PCC | Mntbl | Y | 446 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% |
| Fair | CRB_66S | SDWK_66S | S | Bowker Ave | | Lulie St | 80.9 | PCC | Mntbl | Y | 407 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 11% |
| Good | CRB_67N | SDWK_67N | N | Bowker Ave | | Monteith St | 125.8 | PCC | Mntbl | Y | 656 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_67S | SDWK_67S | S | Bowker Ave | | Monteith St | 123.4 | PCC | Cr_Gtt | Y | 292 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 24% |
| Good | CRB_75N | SDWK_75N | N | Brighton Ave | | Mitchell St | 55.2 | PCC | Cr_Gtt | Y | 465 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 5% |
| Good | CRB_75S | SDWK_75S | S | Brighton Ave | | Mitchell St | 61.6 | PCC | Cr_Gtt | Y | 441 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 10% |
| Good | CRB_76N | SDWK_76N | N | Brighton Ave | | Hewlett Pl | 84.6 | PCC | Cr_Gtt | Y | 275 | N | 36.0 | 0.0 | \$20,880 | \$0 | \$20,880 | 36 | 43% |
| Good | CRB_76S | SDWK_76S | S | Brighton Ave | | Hewlett Pl | 81.0 | PCC | Cr_Gtt | Y | 103 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 11% |
| Fair | CRB_79N | | N | Brighton Ave | | Monterey Ave | 79.9 | AC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_79S | SDWK_79S | S | Brighton Ave | | Monterey Ave | 82.5 | PCC | Cr_Gtt | Y | 498 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Poor | CRB_82N | | N | Broom Rd | | Prospect Pl | 59.9 | PCC | Cr_Gtt | Y | 0 | Y | 60.0 | 0.0 | \$34,800 | \$0 | \$34,800 | 60 | 100% |
| Poor | CRB_82S | | S | Broom Rd | | Prospect Pl | 73.5 | PCC | Cr_Gtt | Y | 0 | Y | 72.0 | 0.0 | \$41,760 | \$0 | \$41,760 | 72 | 98% |
| Poor | CRB_83E | | E | Burdick Ave | | Uplands Rd | 163.8 | PCC | Cr_Gtt | Y | 0 | Y | 165.0 | 0.0 | \$95,700 | \$0 | \$95,700 | 165 | 100% |
| Poor | CRB_83W | | W | Burdick Ave | | Uplands Rd | 162.6 | PCC | Cr_Gtt | Y | 0 | Y | 162.0 | 0.0 | \$93,960 | \$0 | \$93,960 | 162 | 100% |
| Fair | CRB_84E | | E | Burdick Ave | | Nottingham Rd | 220.4 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_84W | | W | Burdick Ave | | Nottingham Rd | 218.9 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_85E | | E | Burdick Ave | | Mayhew St | 120.6 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_85W | SDWK_85W | W | Burdick Ave | | Mayhew St | 129.8 | PCC | Mntbl | Y | 497 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_86N | | N | Burdick Ave | | Musgrave St | 78.6 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_86S | | S | Burdick Ave | | Musgrave St | 84.1 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_87N | | N | Burdick Ave | | Dunlevy St | 84.3 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_87S | | S | Burdick Ave | | Dunlevy St | 85.2 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_88N | | N | Burdick Ave | | Heron St | 84.3 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_88S | | S | Burdick Ave | | Heron St | 84.1 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_93N | SDWK_93N | N | Byron St | | Foul Bay Rd | 235.9 | PCC | Mower | N | 34 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 14% |
| Fair | CRB_93S | SDWK_93S | S | Byron St | | Foul Bay Rd | 238.9 | PCC | Mower | N | 278 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 14% |
| Good | CRB_100N | SDWK_100N | N | Cadboro Bay Rd | | Christie Way | 137.6 | PCC | Cr_Gtt | N | 409 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 7% |

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | | To | Length | Material | Type | Paved | | | SW | | | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent |
|----------------|-----------|-------------|-------|-------------------------|---------------------------------|---------------------------------|--------|----------|--------|--------|----------|-------|----------------|---------------|----------|---------|------------------|------------|------|--|-------------------|----------------|
| | | | Face | Street | From | | | | | Gutter | Priority | Proj? | PCC Length (m) | AC Length (m) | PCC Cost | | AC Cost | Total Cost | | | | |
| Good | CRB_100S | SDWK_100S_B | E | Cadboro Bay Rd | Christie Way | Bowker Ave | 146.0 | PCC | Cr_Gtt | N | 676 | N | 6.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_101E | SDWK_101E | E | Cadboro Bay Rd | Bowker Ave | Dalhousie St | 124.8 | PCC | Mntbl | N | 432 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% | | | |
| Fair | CRB_101W | SDWK_101W | W | Cadboro Bay Rd | Bowker Ave | Dalhousie St | 103.5 | PCC | Cr_Gtt | N | 378 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 12% | | | |
| Good | CRB_103E | SDWK_103E | E | Cadboro Bay Rd | Dalhousie St | Tod Rd / Woodhouse Rd | 134.4 | PCC | Cr_Gtt | N | 296 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 20% | | | |
| Good | CRB_103W | SDWK_103W | W | Cadboro Bay Rd | Dalhousie St | Tod Rd / Woodhouse Rd | 152.1 | PCC | Cr_Gtt | N | 256 | N | 54.0 | 0.0 | \$31,320 | \$0 | \$31,320 | 54 | 36% | | | |
| Good | CRB_104E | SDWK_104E | E | Cadboro Bay Rd | Tod Rd / Woodhouse Rd | Hamiota St | 52.8 | PCC | Cr_Gtt | N | 266 | Y | 42.0 | 0.0 | \$24,360 | \$0 | \$24,360 | 42 | 80% | | | |
| Good | CRB_104W | SDWK_104W | W | Cadboro Bay Rd | Tod Rd / Woodhouse Rd | Hamiota St | 44.6 | PCC | Cr_Gtt | N | 4 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 20% | | | |
| Good | CRB_105E | SDWK_105E | E | Cadboro Bay Rd | Hamiota St | Kinross Ave | 40.9 | PCC | Cr_Gtt | N | 291 | Y | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 73% | | | |
| Good | CRB_105W | SDWK_105W | W | Cadboro Bay Rd | Hamiota St | Kinross Ave | 46.9 | PCC | Cr_Gtt | Y | 304 | Y | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 58% | | | |
| Good | CRB_106E | SDWK_106E | E | Cadboro Bay Rd | Kinross Ave | Estevan Ave | 84.7 | PCC | Cr_Gtt | N | 300 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 32% | | | |
| Good | CRB_106W | SDWK_106W | W | Cadboro Bay Rd | Kinross Ave | Estevan Ave | 81.6 | PCC | Cr_Gtt | N | 447 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% | | | |
| Fair | CRB_107E | SDWK_107E | E | Cadboro Bay Rd | Estevan Ave | Pacific Ave | 85.4 | PCC | Cr_Gtt | N | 87 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 25% | | | |
| Fair | CRB_107W | SDWK_107W | W | Cadboro Bay Rd | Estevan Ave | Pacific Ave | 87.1 | PCC | Cr_Gtt | N | 305 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 31% | | | |
| Fair | CRB_108E | SDWK_108E | E | Cadboro Bay Rd | Pacific Ave | Harlow Dr | 92.0 | PCC | Cr_Gtt | N | 282 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 36% | | | |
| Good | CRB_108W | SDWK_108W | W | Cadboro Bay Rd | Pacific Ave | Harlow Dr | 90.5 | PCC | Cr_Gtt | N | 289 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 33% | | | |
| Fair | CRB_109E | SDWK_109E | E | Cadboro Bay Rd | Harlow Dr | Allenby St | 96.8 | PCC | Cr_Gtt | Y | 375 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 12% | | | |
| Good | CRB_109W | SDWK_109W | W | Cadboro Bay Rd | Harlow Dr | Allenby St | 95.9 | PCC | Cr_Gtt | Y | 324 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 22% | | | |
| Good | CRB_110E | SDWK_110E | E | Cadboro Bay Rd | Allenby St | Thompson Ave | 38.5 | PCC | Cr_Gtt | N | 730 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_110W | SDWK_110W | W | Cadboro Bay Rd | Allenby St | Thompson Ave | 55.0 | PCC | Cr_Gtt | N | 383 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 22% | | | |
| Good | CRB_111E | SDWK_111E | E | Cadboro Bay Rd | Thompson Ave | Neil St | 51.8 | PCC | Cr_Gtt | N | 445 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 12% | | | |
| Fair | CRB_111W | SDWK_111W | W | Cadboro Bay Rd | Thompson Ave | Neil St | 40.2 | PCC | Cr_Gtt | N | 484 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% | | | |
| Fair | CRB_112E | SDWK_112E | E | Cadboro Bay Rd | Neil St | Dufferin Ave | 119.6 | PCC | Cr_Gtt | N | 251 | Y | 63.0 | 0.0 | \$36,540 | \$0 | \$36,540 | 63 | 53% | | | |
| Fair | CRB_112W | SDWK_112W | W | Cadboro Bay Rd | Neil St | Dufferin Ave | 127.5 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_114E | SDWK_114E | E | Cadboro Bay Rd | Uplands Rd | Lansdowne Rd | 284.1 | PCC | Mower | Y | 259 | N | 45.0 | 0.0 | \$26,100 | \$0 | \$26,100 | 45 | 16% | | | |
| Fair | CRB_114W | SDWK_114W | W | Cadboro Bay Rd | Uplands Rd | Lansdowne Rd | 309.1 | PCC | Cr_Gtt | Y | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% | | | |
| Fair | CRB_115E | SDWK_115E | E | Cadboro Bay Rd | Lansdowne Rd | Sheringham Pl | 133.1 | PCC | Cr_Gtt | N | 238 | Y | 123.0 | 0.0 | \$71,340 | \$0 | \$71,340 | 123 | 92% | | | |
| Fair | CRB_115W | SDWK_115W | W | Cadboro Bay Rd | Lansdowne Rd | Sheringham Pl | 134.8 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% | | | |
| Fair | CRB_116E | SDWK_116E | E | Cadboro Bay Rd | Sheringham Pl | Cotswold Rd | 134.1 | PCC | Mower | N | 398 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 7% | | | |
| Fair | CRB_116W | SDWK_116W | W | Cadboro Bay Rd | Sheringham Pl | Cotswold Rd | 141.0 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_117E | SDWK_117E | E | Cadboro Bay Rd | Cotswold Rd | Cardigan Rd | 330.4 | PCC | Mower | N | 247 | N | 84.0 | 0.0 | \$48,720 | \$0 | \$48,720 | 84 | 25% | | | |
| Fair | CRB_117W | SDWK_117W | W | Cadboro Bay Rd | Cotswold Rd | Cardigan Rd | 353.9 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% | | | |
| Fair | CRB_118E | SDWK_118E | E | Cadboro Bay Rd | Cardigan Rd | Chiltern Pl | 122.5 | PCC | Mower | N | 438 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% | | | |
| Fair | CRB_118W | SDWK_118W | W | Cadboro Bay Rd | Cardigan Rd | Chiltern Pl | 127.1 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_119E | SDWK_119EB | E | Cadboro Bay Rd | Chiltern Pl | Cedar Hill Cross Rd / Upper Ter | 329.8 | PCC | Mower | N | 241 | N | 114.0 | 0.0 | \$66,120 | \$0 | \$66,120 | 114 | 35% | | | |
| Fair | CRB_119W | SDWK_119W | W | Cadboro Bay Rd | Chiltern Pl | Cedar Hill Cross Rd / Upper Ter | 347.5 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% | | | |
| Fair | CRB_120E | SDWK_120E | E | Cadboro Bay Rd | Cedar Hill Cross Rd / Upper Ter | Beach Dr | 288.7 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_120W | SDWK_120W | W | Cadboro Bay Rd | Cedar Hill Cross Rd / Upper Ter | Beach Dr | 297.8 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_121E | SDWK_121E | E | Cadboro Bay Rd | Beach Dr | Hibbens Cs | 20.5 | PCC | Cr_Gtt | N | 379 | Y | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 59% | | | |
| Good | CRB_121W | SDWK_121W | W | Cadboro Bay Rd | Beach Dr | Hibbens Cs | 35.6 | PCC | Mower | N | 666 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_122W | SDWK_122W | W | Cadboro Bay Rd | Hibbens Cs | North End | 39.9 | PCC | Cr_Gtt | N | 357 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 38% | | | |
| Good | CRB_95N | SDWK_95N_A | N | Cadboro Bay Rd | Foul Bay Rd | Bee St / Florence St | 84.3 | PCC | Cr_Gtt | N | 135 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% | | | |
| Good | CRB_95S | SDWK_95S | S | Cadboro Bay Rd | Foul Bay Rd | Bee St / Florence St | 91.8 | PCC | Cr_Gtt | N | 560 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Poor | CRB_96N | SDWK_96N | N | Cadboro Bay Rd | Bee St / Florence St | Epworth St | 91.5 | PCC | Cr_Gtt | Y | 222 | Y | 93.0 | 0.0 | \$53,940 | \$0 | \$53,940 | 93 | 100% | | | |
| Good | CRB_96S | SDWK_96S | S | Cadboro Bay Rd | Bee St / Florence St | Epworth St | 89.8 | PCC | Cr_Gtt | Y | 551 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Good | CRB_97N | SDWK_97N | N | Cadboro Bay Rd | Epworth St | Cranmore Rd | 130.4 | PCC | Cr_Gtt | N | 362 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 9% | | | |
| Good | CRB_97S | SDWK_97S | S | Cadboro Bay Rd | Epworth St | Cranmore Rd | 118.1 | PCC | Cr_Gtt | N | 355 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 13% | | | |
| Fair | CRB_98N | SDWK_98N | N | Cadboro Bay Rd | Cranmore Rd | Eastdowne Rd | 140.3 | PCC | Mntbl | N | 458 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% | | | |
| Good | CRB_98S | SDWK_98S | S | Cadboro Bay Rd | Cranmore Rd | Eastdowne Rd | 136.8 | PCC | Cr_Gtt | N | 414 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% | | | |
| Good | CRB_99N | SDWK_99N | N | Cadboro Bay Rd | Eastdowne Rd | Christie Way | 92.4 | PCC | Cr_Gtt | N | 483 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | | | |
| Good | CRB_99S | SDWK_99S | S | Cadboro Bay Rd | Eastdowne Rd | Christie Way | 97.8 | PCC | Cr_Gtt | N | 418 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 6% | | | |
| Fair | CRB_123E | SDWK_123E | E | Cardiff Pl | South End | Kendal Ave | 90.2 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | | | |
| Fair | CRB_123W | SDWK_123W | W | Cardiff Pl | South End | Kendal Ave | 96.1 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | | | |
| Fair | CRB_124E | SDWK_124E | E | Cardiff Pl | Kendal Ave | Renfrew Rd | 109.8 | PCC | Mntbl | N | 122 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | | | |
| Fair | CRB_124W | SDWK_124W | W | Cardiff Pl | Kendal Ave | Renfrew Rd | 105.1 | PCC | Mntbl | N | 137 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_125E | SDWK_125E | E | Cardiff Pl | Renfrew Rd | Avondale Rd | 171.4 | PCC | Mntbl | N | 400 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 5% | | | |
| Fair | CRB_125W | SDWK_125W | W | Cardiff Pl | Renfrew Rd | Avondale Rd | 163.5 | PCC | Mntbl | N | 306 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 17% | | | |
| Fair | CRB_126E | SDWK_126E | E | Cardiff Pl | Avondale Rd | Redwood Ave | 118.1 | PCC | Mntbl | Y | 264 | N | 42.0 | 0.0 | \$24,360 | \$0 | \$24,360 | 42 | 36% | | | |
| Fair | CRB_126W | SDWK_126W | W | Cardiff Pl | Avondale Rd | Redwood Ave | 104.6 | PCC | Cr_Gtt | Y | 302 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 26% | | | |
| Fair | CRB_127E | SDWK_127E | E | Cardiff Pl | Redwood Ave | Swanson Pl | 141.2 | PCC | Mntbl | N | 50 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 8% | | | |
| Fair | CRB_127W | SDWK_127W | W | Cardiff Pl | Redwood Ave | Swanson Pl | 130.3 | PCC | Mntbl | N | 123 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 21% | | | |
| Fair | CRB_128E | SDWK_128E | E | Cardiff Pl | Swanson Pl | Frederick Norris Rd | 74.5 | PCC | Mntbl | N | 651 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_128W | SDWK_128W | W | Cardiff Pl | Swanson Pl | Frederick Norris Rd | 74.0 | PCC | Mntbl | N | 76 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 41% | | | |
| Fair | CRB_129N | SDWK_129N | N | Cardigan Rd | Cadboro Bay Rd | Uplands Rd | 211.7 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_129S | SDWK_129S | S | Cardigan Rd | Cadboro Bay Rd | Uplands Rd | 199.8 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_130N | SDWK_130N | N | Carnarvon St | Foul Bay Rd | Henderson Rd | 225.7 | PCC | Mntbl | N | 717 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_130S | SDWK_130S | S | Carnarvon St | Foul Bay Rd | Henderson Rd | 226.8 | PCC | Mntbl | N | 694 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_131N | SDWK_131N | N | Carrick St | Foul Bay Rd | Henderson Rd / Wootton Cr | 248.6 | PCC | Mntbl | N | 294 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 12% | | | |
| Fair | CRB_131S | SDWK_131S | S | Carrick St | Foul Bay Rd | Henderson Rd / Wootton Cr | 250.8 | PCC | Mntbl | N | 644 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Poor | CRB_132EA | SDWK_132EA | E | Cattle Point Lookout Rd | Beach Dr | Beach Dr | 438.2 | PCC | Cr_Gtt | N | 387 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 2% | | | |
| Fair | CRB_132EB | SDWK_132EB | E | Cattle Point Lookout Rd | Beach Dr | Beach Dr | 275.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_132NF | SDWK_132NF | N | Cattle Point Lookout Rd | Beach Dr | Beach Dr | 106.6 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_132SC | SDWK_132SC | S | Cattle Point Lookout Rd | Beach Dr | Beach Dr | 64.5 | PCC | Cr_Gtt | N | 433 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 9% | | | |
| Fair | CRB_132WB | SDWK_132WB | W | Cattle Point Lookout Rd | Beach Dr | Beach Dr | 105.6 | PCC | Cr_Gtt | N | 689 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_132WE | SDWK_132WE | W | Cattle Point Lookout Rd | Beach Dr | Beach Dr | 303.1 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | | | |
| Fair | CRB_133N | SDWK_133N | N | Cavendish Ave | Hampshire Rd / Musgrave St | Beach Dr | 368.6 | PCC | Mnt | | | | | | | | | | | | | |

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | Block SW ID | Block Face | Street | From | To | Length | Material | Type | Paved Gutter | SW Priority | Capital Proj? | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | |
|----------------|-----------|-------------|------------|---------------------|------------------------------|------------------------------|--------|----------|--------|--------------|-------------|---------------|------------------|---------------|----------|----------|-------------------|----------------|------------|
| | | | | | | | | | | | | | PCC Length (m) | AC Length (m) | PCC Cost | AC Cost | | | Total Cost |
| Fair | CRB_135N | | N | Cedar Hill Cross Rd | Gordon Head Rd | Henderson Rd / University Dr | 426.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_135S | SDWK_135S | S | Cedar Hill Cross Rd | Gordon Head Rd | Henderson Rd / University Dr | 464.3 | PCC | Cr_Gtt | N | 767 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_136NA | | N | Cedar Hill Cross Rd | Henderson Rd / University Dr | Crestview Rd | 45.4 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_136NB | | N | Cedar Hill Cross Rd | Henderson Rd / University Dr | Crestview Rd | 857.2 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_136SA | SDWK_136SA | S | Cedar Hill Cross Rd | Henderson Rd / University Dr | Crestview Rd | 38.6 | PCC | Cr_Gtt | N | 308 | Y | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 70% |
| Fair | CRB_136SB | SDWK_136SB | S | Cedar Hill Cross Rd | Henderson Rd / University Dr | Crestview Rd | 355.0 | PCC | Cr_Gtt | N | 311 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 7% |
| Fair | CRB_136SC | SDWK_136SC | S | Cedar Hill Cross Rd | Henderson Rd / University Dr | Crestview Rd | 528.6 | PCC | Cr_Gtt | N | 671 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_137N | | N | Cedar Hill Cross Rd | Crestview Rd | Cadboro Bay Rd / Upper Ter | 141.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_137S | SDWK_137S | S | Cedar Hill Cross Rd | Crestview Rd | Cadboro Bay Rd / Upper Ter | 121.6 | PCC | Cr_Gtt | N | 729 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_138N | SDWK_138N | N | Central Ave | Mountjoy Ave | Falkland Rd | 98.2 | PCC | Mntbl | N | 88 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_138S | SDWK_138S | S | Central Ave | Mountjoy Ave | Falkland Rd | 102.7 | PCC | Mntbl | N | 100 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_139N | SDWK_139N | N | Central Ave | Falkland Rd | Victoria Ave | 151.9 | PCC | Mntbl | N | 189 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_139S | SDWK_139S | S | Central Ave | Falkland Rd | Victoria Ave | 148.2 | PCC | Cr_Gtt | N | 206 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_140N | SDWK_140N | N | Central Ave | Victoria Ave | Hampshire Rd | 130.7 | PCC | Mntbl | N | 630 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_140S | SDWK_140S | S | Central Ave | Victoria Ave | Hampshire Rd | 128.6 | PCC | Mntbl | N | 634 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_141N | SDWK_141N | N | Central Ave | Hampshire Rd | Monterey Ave | 76.1 | PCC | Mntbl | N | 719 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_141S | SDWK_141S | S | Central Ave | Hampshire Rd | Monterey Ave | 74.3 | PCC | Mower | N | 186 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_142N | SDWK_142N | N | Central Ave | Monterey Ave | Oliver St | 125.7 | PCC | Mntbl | Y | 683 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_142S | SDWK_142S | S | Central Ave | Monterey Ave | Oliver St | 126.9 | PCC | Mntbl | N | 205 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_143N | SDWK_143N | N | Central Ave | Oliver St | St Patrick St | 89.1 | PCC | Mntbl | N | 599 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_143S | SDWK_143S | S | Central Ave | Oliver St | St Patrick St | 89.2 | PCC | Mntbl | N | 702 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_144N | SDWK_144N | N | Central Ave | St Patrick St | Lyn Cr | 84.7 | PCC | Mntbl | N | 18 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_144S | SDWK_144S | S | Central Ave | St Patrick St | Lyn Cr | 85.1 | PCC | Mntbl | N | 521 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_145N | SDWK_145N | N | Central Ave | Lyn Cr | Transit Rd | 63.2 | PCC | Mntbl | N | 121 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_145S | SDWK_145S | S | Central Ave | Lyn Cr | Transit Rd | 59.1 | PCC | Mntbl | N | 602 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_146N | SDWK_146N | N | Central Ave | Transit Rd | Island Rd | 108.1 | PCC | Mntbl | N | 51 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_146S | SDWK_146S | S | Central Ave | Transit Rd | Island Rd | 117.9 | PCC | Mntbl | N | 749 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_147N | SDWK_147N | N | Central Ave | Island Rd | Byng St | 133.0 | PCC | Mntbl | N | 26 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_147S | SDWK_147S | S | Central Ave | Island Rd | Byng St | 149.3 | PCC | Mntbl | N | 175 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_148N | SDWK_148N | N | Central Ave | Byng St | Linkleas Ave | 76.4 | PCC | Mntbl | N | 99 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_148S | SDWK_148S | S | Central Ave | Byng St | Linkleas Ave | 81.7 | PCC | Mntbl | N | 202 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_149N | SDWK_149N | N | Central Ave | Linkleas Ave | Newport Ave | 70.6 | PCC | Mntbl | N | 64 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_149S | SDWK_149S | S | Central Ave | Linkleas Ave | Newport Ave | 65.3 | PCC | Mntbl | N | 185 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_151N | SDWK_151N | N | Chaucer St | Foul Bay Rd | Elgin Rd | 238.0 | PCC | Mower | N | 8 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 14% |
| Fair | CRB_151S | SDWK_151S | S | Chaucer St | Foul Bay Rd | Elgin Rd | 237.5 | PCC | Mower | N | 48 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 5% |
| Good | CRB_152E | | E | Chiltern Pl | Cadboro Bay Rd | Southeast End | 97.8 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_152W | | W | Chiltern Pl | Cadboro Bay Rd | Southeast End | 98.4 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_153E | SDWK_153E | E | Christie Way | Cranmore Rd | Cadboro Bay Rd | 115.1 | PCC | Cr_Gtt | N | 293 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 26% |
| Fair | CRB_153W | SDWK_153W | W | Christie Way | Cranmore Rd | Cadboro Bay Rd | 117.2 | PCC | Cr_Gtt | N | 450 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% |
| Fair | CRB_155N | | N | Cookman St | St Patrick St | Transit Rd | 144.9 | AC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_155S | | S | Cookman St | St Patrick St | Transit Rd | 141.5 | AC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_156N | | N | Cotswold Rd | Cadboro Bay Rd | Uplands Rd | 205.4 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_156S | | S | Cotswold Rd | Cadboro Bay Rd | Uplands Rd | 202.4 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_157N | | N | Cotswold Rd | Uplands Rd | Upper Ter / Weald Rd | 201.0 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_157S | SDWK_157S | S | Cotswold Rd | Uplands Rd | Upper Ter / Weald Rd | 208.7 | PCC | Mower | Y | 152 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_159N | SDWK_159N | N | Cranmore Rd | Cadboro Bay Rd | Christie Way | 179.0 | PCC | Mntbl | N | 232 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 7% |
| Good | CRB_159S | SDWK_159S | S | Cranmore Rd | Cadboro Bay Rd | Christie Way | 210.7 | PCC | Mntbl | N | 371 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 6% |
| Fair | CRB_160S | SDWK_160S | S | Cranmore Rd | Christie Way | Armstrong Ave | 31.9 | PCC | Mntbl | Y | 607 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_161N | SDWK_161N | N | Cranmore Rd | Armstrong Ave | Hampshire Rd | 156.8 | PCC | Mntbl | Y | 511 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_161S | SDWK_161S | S | Cranmore Rd | Armstrong Ave | Hampshire Rd | 115.2 | PCC | Mntbl | Y | 579 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_162N | | N | Cranmore Rd | Hampshire Rd | St Ann St | 97.4 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_162S | SDWK_162S | S | Cranmore Rd | Hampshire Rd | St Ann St | 102.4 | PCC | Cr_Gtt | N | 406 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 9% |
| Fair | CRB_163N | | N | Cranmore Rd | St Ann St | Lullie St | 82.0 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_163S | SDWK_163S | S | Cranmore Rd | St Ann St | Lullie St | 82.5 | PCC | Mntbl | Y | 212 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_164N | | N | Cranmore Rd | Lullie St | Monteith St | 80.2 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_164S | SDWK_164S | S | Cranmore Rd | Lullie St | Monteith St | 79.1 | PCC | Mntbl | N | 188 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_165N | | N | Cranmore Rd | Monteith St | Beach Dr | 148.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_165S | SDWK_165S | S | Cranmore Rd | Monteith St | Beach Dr | 156.8 | PCC | Mntbl | N | 761 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_167E | | E | Crescent Rd | Penzance Rd | Repulse St | 53.8 | AC | Mntbl | Y | 0 | Y | 0.0 | 54.0 | \$20,520 | \$20,520 | \$20,520 | 54 | 100% |
| Poor | CRB_167W | | W | Crescent Rd | Penzance Rd | Repulse St | 55.4 | AC | Mntbl | Y | 0 | Y | 0.0 | 44.0 | \$16,720 | \$16,720 | \$16,720 | 44 | 79% |
| Poor | CRB_168E | | E | Crescent Rd | Repulse St | Quimper St | 53.6 | AC | Mntbl | Y | 0 | Y | 0.0 | 54.0 | \$20,520 | \$20,520 | \$20,520 | 54 | 100% |
| Good | CRB_168W | | W | Crescent Rd | Repulse St | Quimper St | 99.2 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_169E | | E | Crescent Rd | Quimper St | Lorne Ter | 104.1 | AC | Mntbl | Y | 0 | Y | 0.0 | 104.0 | \$39,520 | \$39,520 | \$39,520 | 104 | 100% |
| Good | CRB_169W | | W | Crescent Rd | Quimper St | Lorne Ter | 109.9 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_170E | SDWK_170E | E | Crescent Rd | Lorne Ter | King George Ter | 26.8 | PCC | Mower | N | 354 | Y | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 56% |
| Fair | CRB_170W | SDWK_170W | W | Crescent Rd | Lorne Ter | King George Ter | 27.9 | PCC | Mntbl | N | 353 | Y | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 54% |
| Good | CRB_171E | | E | Crescent Rd | King George Ter | Foul Bay Rd | 250.7 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_171W | SDWK_171W | W | Crescent Rd | King George Ter | Foul Bay Rd | 251.3 | PCC | Mntbl | N | 405 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 4% |
| Fair | CRB_5000W | SDWK_5000W | W | Crescent Rd | Crescent Rd | Foul Bay Rd | 18.7 | PCC | Cr_Gtt | N | 552 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_172E | SDWK_172E | E | Crestview Rd | Cedar Hill Cross Rd | North End | 347.4 | PCC | Mntbl | N | 725 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_172W | SDWK_172W | W | Crestview Rd | Cedar Hill Cross Rd | North End | 338.4 | PCC | Mower | N | 680 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_174E | | E | Currie Rd | Transit Rd | Island Rd | 112.0 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_174S | SDWK_174S | S | Currie Rd | Transit Rd | Island Rd | 99.1 | PCC | Cr_Gtt | N | 752 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_175N | | N | Currie Rd | Island Rd | St Louis St | 168.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_175S | SDWK_175S | S | Currie Rd | Island Rd | St Louis St | 127.7 | PCC | Cr_Gtt | N | 606 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_176S | SDWK_176S | S | Currie Rd | St Louis St | Newport Ave | 123.1 | PCC | Cr_Gtt | N | 469 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | Block | | From | To | Length | Material | Type | Paved | | | Curb Replacement | | | Recon. Length (m) | Recon. Percent | | |
|----------------|-----------|-----------|------|--------------|----------------|--------|----------|--------|--------|--------|----------|------------------|----------------|---------------|-------------------|----------------|----------|---------|
| | | SW ID | Face | | | | | | Street | Gutter | Priority | Proj? | PCC Length (m) | AC Length (m) | | | PCC Cost | AC Cost |
| Good | CRB_176W | | W | Currie Rd | St Louis St | 129.4 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_177N | SDWK_177N | N | Currie Rd | Newport Ave | 79.7 | PCC | Mntbl | N | 723 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_177S | SDWK_177S | S | Currie Rd | Newport Ave | 84.6 | PCC | Mntbl | N | 601 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_178N | | N | Currie Rd | Goodwin St | 77.7 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_178S | SDWK_178S | S | Currie Rd | Goodwin St | 87.5 | PCC | Mntbl | N | 627 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_179N | SDWK_179N | N | Dalhousie St | Eastdowne Rd | 75.4 | PCC | Cr_Gtt | Y | 104 | Y | 75.0 | 0.0 | \$43,500 | \$0 | \$43,500 | 75 | 99% |
| Fair | CRB_179S | SDWK_179S | S | Dalhousie St | Eastdowne Rd | 76.6 | PCC | Cr_Gtt | Y | 157 | Y | 48.0 | 0.0 | \$27,840 | \$0 | \$27,840 | 48 | 63% |
| Fair | CRB_180N | SDWK_180N | N | Dalhousie St | Mowat St | 77.4 | PCC | Cr_Gtt | Y | 301 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 35% |
| Fair | CRB_180S | SDWK_180S | S | Dalhousie St | Mowat St | 88.3 | PCC | Cr_Gtt | Y | 270 | N | 39.0 | 0.0 | \$22,620 | \$0 | \$22,620 | 39 | 44% |
| Good | CRB_181N | SDWK_181N | N | Dalhousie St | Dryfe St | 77.2 | PCC | Cr_Gtt | Y | 457 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_181S | SDWK_181S | S | Dalhousie St | Dryfe St | 80.4 | PCC | Cr_Gtt | Y | 326 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 26% |
| Good | CRB_182N | SDWK_182N | N | Dalhousie St | Cadboro Bay Rd | 191.0 | PCC | Cr_Gtt | N | 770 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_182S | SDWK_182S | S | Dalhousie St | Cadboro Bay Rd | 185.9 | PCC | Cr_Gtt | N | 771 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_183N | SDWK_183N | N | Dalhousie St | Musgrave St | 84.0 | PCC | Mntbl | N | 640 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_183S | SDWK_183S | S | Dalhousie St | Musgrave St | 88.9 | PCC | Mntbl | N | 743 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_184N | SDWK_184N | N | Dalhousie St | Dunlevy St | 267.6 | PCC | Mntbl | N | 490 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_184S | SDWK_184S | S | Dalhousie St | Dunlevy St | 272.4 | PCC | Mntbl | N | 753 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_185EC | | E | Dalhousie St | Beach Dr | 20.2 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_185NA | | N | Dalhousie St | Beach Dr | 103.3 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_185SA | | S | Dalhousie St | Beach Dr | 87.0 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_185SB | | S | Dalhousie St | Beach Dr | 59.8 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_186E | SDWK_186E | E | Deal St | South End | 320.0 | PCC | Mntbl | N | 388 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 3% |
| Good | CRB_190E | | E | Devon Rd | South End | 47.5 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 6% |
| Fair | CRB_190W | SDWK_190W | W | Devon Rd | South End | 69.6 | PCC | Mntbl | N | 214 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_191E | | E | Devon Rd | Dover Rd | 260.5 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_191W | SDWK_191W | W | Devon Rd | Dover Rd | 236.9 | PCC | Mntbl | N | 201 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_192E | | E | Dewdney Ave | Uplands Rd | 163.1 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_192W | | W | Dewdney Ave | Uplands Rd | 164.0 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_193E | | E | Dewdney Ave | Nottingham Rd | 224.3 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_193W | | W | Dewdney Ave | Nottingham Rd | 219.5 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_194E | | E | Dewdney Ave | Mayhew St | 80.6 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_194W | | W | Dewdney Ave | Mayhew St | 80.2 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_195E | | E | Dorset Rd | Nottingham Rd | 269.2 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_195W | SDWK_195W | W | Dorset Rd | Nottingham Rd | 268.5 | PCC | Mntbl | N | 513 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_196N | | N | Dorset Rd | Musgrave St | 23.5 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_196S | | S | Dorset Rd | Musgrave St | 31.0 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_201E | SDWK_201E | E | Dover Rd | Nottingham Rd | 170.0 | PCC | Mntbl | N | 174 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_201W | | W | Dover Rd | Nottingham Rd | 176.7 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_202E | SDWK_202E | E | Dryfe St | Dalhousie St | 147.2 | PCC | Mntbl | N | 772 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_202W | SDWK_202W | W | Dryfe St | Dalhousie St | 149.6 | PCC | Mntbl | N | 209 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_203E | SDWK_203E | E | Dufferin Ave | Cadboro Bay Rd | 160.9 | PCC | Mntbl | N | 154 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_203W | SDWK_203W | W | Dufferin Ave | Cadboro Bay Rd | 139.3 | PCC | Mntbl | N | 134 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_204E | SDWK_204E | E | Dufferin Ave | Nottingham Rd | 220.9 | PCC | Mntbl | N | 657 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_204W | SDWK_204W | W | Dufferin Ave | Nottingham Rd | 226.0 | PCC | Mntbl | N | 161 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_205E | SDWK_205E | E | Dufferin Ave | Mayhew St | 171.4 | PCC | Cr_Gtt | N | 773 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_205W | SDWK_205W | W | Dufferin Ave | Mayhew St | 178.4 | PCC | Mntbl | N | 423 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_206N | | N | Dundrum Rd | Henderson Rd | 129.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_206S | | S | Dundrum Rd | Henderson Rd | 128.3 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_207E | SDWK_207E | E | Dunlevy St | Dalhousie St | 300.2 | PCC | Mower | N | 78 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_207W | SDWK_207W | W | Dunlevy St | Dalhousie St | 300.9 | PCC | Cr_Gtt | N | 755 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_208E | SDWK_208E | E | Dunlevy St | Estevan Ave | 218.0 | PCC | Mntbl | Y | 170 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_208W | SDWK_208W | W | Dunlevy St | Estevan Ave | 217.0 | PCC | Mntbl | Y | 124 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_209E | SDWK_209E | E | Dunlevy St | Burdick Ave | 217.6 | PCC | Mntbl | N | 142 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_209W | SDWK_209W | W | Dunlevy St | Burdick Ave | 216.5 | PCC | Mntbl | N | 147 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_210E | SDWK_210E | E | Eastdowne Rd | Cadboro Bay Rd | 66.9 | PCC | Cr_Gtt | N | 487 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_210W | SDWK_210W | W | Eastdowne Rd | Cadboro Bay Rd | 74.1 | PCC | Mntbl | N | 117 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 20% |
| Good | CRB_212E | SDWK_212E | E | Eastdowne Rd | Cranleigh Pl | 73.5 | PCC | Cr_Gtt | Y | 345 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 20% |
| Fair | CRB_212W | SDWK_212W | W | Eastdowne Rd | Cranleigh Pl | 72.3 | PCC | Mntbl | Y | 133 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_213E | SDWK_213E | E | Eastdowne Rd | Dalhousie St | 26.1 | PCC | Cr_Gtt | N | 577 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_213W | SDWK_213W | W | Eastdowne Rd | Dalhousie St | 23.4 | PCC | Cr_Gtt | N | 496 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 13% |
| Fair | CRB_214E | SDWK_214E | E | Eastdowne Rd | Fair St | 117.6 | PCC | Mntbl | N | 337 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 15% |
| Fair | CRB_214W | SDWK_214W | W | Eastdowne Rd | Fair St | 91.9 | PCC | Mntbl | N | 372 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 13% |
| Fair | CRB_215E | SDWK_215E | E | Eastdowne Rd | Haultain St | 43.0 | PCC | Mntbl | Y | 315 | Y | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 56% |
| Fair | CRB_215W | SDWK_215W | W | Eastdowne Rd | Haultain St | 42.4 | PCC | Mntbl | Y | 668 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_216E | SDWK_216E | E | Eastdowne Rd | Kinross Ave | 94.6 | PCC | Mntbl | Y | 221 | Y | 96.0 | 0.0 | \$55,680 | \$0 | \$55,680 | 96 | 100% |
| Fair | CRB_216W | SDWK_216W | W | Eastdowne Rd | Kinross Ave | 100.4 | PCC | Mntbl | Y | 667 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_217E | SDWK_217E | E | Eastdowne Rd | Estevan Ave | 51.2 | PCC | Mntbl | N | 413 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 18% |
| Fair | CRB_217W | SDWK_217W | W | Eastdowne Rd | Estevan Ave | 83.7 | PCC | Mntbl | Y | 633 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_218E | SDWK_218E | E | Eastdowne Rd | Newton St | 76.1 | PCC | Mntbl | N | 660 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_218W | SDWK_218W | W | Eastdowne Rd | Newton St | 73.9 | PCC | Cr_Gtt | N | 610 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_219E | SDWK_219E | E | Eastdowne Rd | Pacific Ave | 86.3 | PCC | Mntbl | N | 662 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_219W | SDWK_219W | W | Eastdowne Rd | Pacific Ave | 95.3 | PCC | Mntbl | N | 645 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_220E | SDWK_220E | E | Eastdowne Rd | Harlow Dr | 101.9 | PCC | Mntbl | N | 774 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_220W | SDWK_220W | W | Eastdowne Rd | Harlow Dr | 108.0 | PCC | Mntbl | N | 724 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_221E | SDWK_221E | E | Eastdowne Rd | Allenby St | 86.2 | PCC | Mntbl | N | 734 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_221W | SDWK_221W | W | Eastdowne Rd | Allenby St | 80.6 | PCC | Mntbl | N | 673 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |

**DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY**

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent |
|----------------|------------|-------------|-------|---------------|-----------------------------|-----------------------|-------|--------|----------|------|-------|----|---------|------------------|----------|---------|------------|-------------------|----------------|
| | | | Face | Street | Street | | | | | | | | | Street | PCC Cost | AC Cost | Total Cost | | |
| Fair | CRB_222E | SDWK_222E | E | Eastdowne Rd | Neil St | Sandowne Rd | 94.4 | PCC | Mntbl | N | 196 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_222W | SDWK_222W | W | Eastdowne Rd | Neil St | Sandowne Rd | 90.8 | PCC | Mntbl | N | 744 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_223E_A | SDWK_223E_A | E | Eastdowne Rd | Sandowne Rd | Middowne Rd | 66.2 | PCC | Mntbl | N | 661 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_223E_B | SDWK_223E_B | E | Eastdowne Rd | Sandowne Rd | Middowne Rd | 88.7 | PCC | Mntbl | N | 775 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_224E | SDWK_224E | E | Eastdowne Rd | Middowne Rd | Oakdowne Rd | 118.7 | PCC | Mntbl | N | 688 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_224W | SDWK_224W | W | Eastdowne Rd | Middowne Rd | Oakdowne Rd | 108.0 | PCC | Mntbl | N | 190 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_225E | SDWK_225E | E | Eastdowne Rd | Oakdowne Rd | Lansdowne Rd | 98.9 | PCC | Mntbl | N | 685 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_225W | SDWK_225W | W | Eastdowne Rd | Oakdowne Rd | Lansdowne Rd | 91.7 | PCC | Mntbl | N | 746 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_226NS | | NS | Edgecliffe Pl | Foul Bay Rd | Southeast End | 217.8 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_227E | SDWK_227E | E | Elgin Rd | Oak Bay Ave | Chaucer St | 69.1 | PCC | Cr_Gtt | Y | 477 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_227W | SDWK_227W | W | Elgin Rd | Oak Bay Ave | Chaucer St | 64.8 | PCC | Cr_Gtt | Y | 733 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_228E | SDWK_228E | E | Elgin Rd | Chaucer St | Byron St | 96.6 | PCC | Mntbl | Y | 368 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 12% |
| Good | CRB_228W | SDWK_228W | W | Elgin Rd | Chaucer St | Byron St | 86.7 | PCC | Mntbl | Y | 12 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_229E | SDWK_229E | E | Elgin Rd | Byron St | Milton St | 94.5 | PCC | Cr_Gtt | Y | 312 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 25% |
| Good | CRB_229W | SDWK_229W | W | Elgin Rd | Byron St | Milton St | 82.7 | PCC | Mntbl | Y | 317 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 29% |
| Good | CRB_230E | | E | Elgin Rd | Milton St | North End | 66.3 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 5% |
| Good | CRB_230W | SDWK_230W | W | Elgin Rd | Milton St | North End | 79.9 | PCC | Mntbl | Y | 480 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_231E | SDWK_231E | E | Epworth St | Cadboro Bay Rd | Fair St | 180.4 | PCC | Mower | N | 453 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_231W | SDWK_231W | W | Epworth St | Cadboro Bay Rd | Fair St | 186.1 | PCC | Mower | N | 25 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_232E | SDWK_232E | E | Epworth St | Fair St | Haultain St | 156.0 | PCC | Mower | N | 463 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Good | CRB_232W | SDWK_232W | W | Epworth St | Fair St | Haultain St | 162.4 | PCC | Mower | N | 454 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_233W | | W | Esplanade | Bowker Ave | Northeast End | 153.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_234E | SDWK_234E | E | Esplanade | Southwest End | Estevan Ave | 161.6 | PCC | Cr_Gtt | Y | 525 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_235E | SDWK_235E | E | Esplanade | Estevan Ave | Northeast End | 343.1 | PCC | Cr_Gtt | Y | 10 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_236N | SDWK_236N | N | Estevan Ave | Eastdowne Rd | Cadboro Bay Rd | 226.5 | PCC | Mntbl | N | 647 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_236S | SDWK_236S | S | Estevan Ave | Eastdowne Rd | Cadboro Bay Rd | 249.2 | PCC | Mntbl | N | 628 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_237N | SDWK_237N | N | Estevan Ave | Cadboro Bay Rd | Hamiota St | 302.7 | PCC | Mntbl | N | 636 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_237S | SDWK_237S | S | Estevan Ave | Cadboro Bay Rd | Hamiota St | 293.5 | PCC | Mntbl | N | 373 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 4% |
| Good | CRB_238N | SDWK_238N | N | Estevan Ave | Hamiota St | Musgrave St | 28.4 | PCC | Mntbl | N | 701 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_238S | SDWK_238S | S | Estevan Ave | Hamiota St | Musgrave St | 20.5 | PCC | Cr_Gtt | N | 776 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_239N | SDWK_239N | N | Estevan Ave | Musgrave St | Dunlevy St | 84.9 | PCC | Cr_Gtt | Y | 11 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_239S | SDWK_239S | S | Estevan Ave | Musgrave St | Dunlevy St | 81.6 | PCC | Cr_Gtt | Y | 349 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 18% |
| Fair | CRB_240N | SDWK_240N | N | Estevan Ave | Dunlevy St | Heron St | 85.2 | PCC | Mntbl | Y | 145 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Poor | CRB_240S | SDWK_240S | S | Estevan Ave | Dunlevy St | Heron St | 83.8 | PCC | Mower | Y | 55 | Y | 84.0 | 0.0 | \$48,720 | \$0 | \$48,720 | 84 | 100% |
| Fair | CRB_241N | SDWK_241N | N | Estevan Ave | Heron St | Lincoln Rd | 87.1 | PCC | Mntbl | N | 90 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_241S | SDWK_241S | S | Estevan Ave | Heron St | Lincoln Rd | 85.5 | PCC | Mntbl | N | 708 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_242N | SDWK_242N | N | Estevan Ave | Lincoln Rd | Beach Dr | 84.7 | PCC | Mntbl | N | 61 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_242S | SDWK_242S | S | Estevan Ave | Lincoln Rd | Beach Dr | 82.8 | PCC | Mntbl | Y | 585 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_243N | | N | Estevan Ave | Beach Dr | Esplanade | 98.6 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_243S | SDWK_243S | S | Estevan Ave | Beach Dr | Esplanade | 102.7 | PCC | Cr_Gtt | N | 605 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_244E | SDWK_244E | E | Exeter Rd | Lansdowne Rd | Beach Dr | 511.1 | PCC | Mntbl | N | 86 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 4% |
| Fair | CRB_244W | | W | Exeter Rd | Lansdowne Rd | Beach Dr | 535.3 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_245N | SDWK_245N | N | Fair St | Epworth St | Eastdowne Rd | 243.6 | PCC | Cr_Gtt | N | 678 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_245S | SDWK_245S | S | Fair St | Epworth St | Eastdowne Rd | 238.4 | PCC | Mntbl | N | 631 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_246N | SDWK_246E | N | Fairfield Rd | West End | Beach Dr / Denison Rd | 11.7 | PCC | Mntbl | Y | 777 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_246W | SDWK_246W | W | Fairfield Rd | West End | Beach Dr / Denison Rd | 32.7 | PCC | Mower | Y | 182 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_251E | | E | Falkland Rd | Central Ave | Pentland Rd | 281.5 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_251W | | W | Falkland Rd | Central Ave | Pentland Rd | 282.9 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_252E | | E | Falkland Rd | Pentland Rd | McNeill Ave | 107.7 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_252W | | W | Falkland Rd | Pentland Rd | McNeill Ave | 113.2 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_258E | SDWK_258E | E | Florence St | Bee St / Cadboro Bay Rd | Haultain St | 365.4 | PCC | Cr_Gtt | N | 778 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_258W | SDWK_258W | W | Florence St | Bee St / Cadboro Bay Rd | Haultain St | 368.1 | PCC | Cr_Gtt | N | 779 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_259E | | E | Foul Bay Rd | South End | Crescent Rd | 36.0 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_259W | | W | Foul Bay Rd | South End | Crescent Rd | 45.2 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_260E | | E | Foul Bay Rd | Crescent Rd | Northwest End | 154.2 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_260W | SDWK_260W | W | Foul Bay Rd | Crescent Rd | Northwest End | 151.8 | PCC | Mower | Y | 472 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Good | CRB_262E | SDWK_262E | E | Foul Bay Rd | McNeill Ave / Richardson St | Quamichan St | 46.6 | PCC | Cr_Gtt | Y | 578 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_263E | SDWK_263E | E | Foul Bay Rd | Quamichan St | Edgecliffe Pl | 70.8 | PCC | Cr_Gtt | Y | 394 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 13% |
| Good | CRB_264E | SDWK_264E | E | Foul Bay Rd | Edgecliffe Pl | Brighton Ave | 338.9 | PCC | Mower | Y | 279 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 10% |
| Good | CRB_265E | SDWK_265E | E | Foul Bay Rd | Brighton Ave | Granite St | 96.9 | PCC | Mower | Y | 395 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 9% |
| Good | CRB_266E | SDWK_266E | E | Foul Bay Rd | Granite St | Oak Bay Ave | 141.3 | PCC | Mntbl | Y | 299 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 19% |
| Fair | CRB_267E | SDWK_267E | E | Foul Bay Rd | Oak Bay Ave | Chaucer St | 80.7 | PCC | Cr_Gtt | N | 393 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 11% |
| Good | CRB_268E | SDWK_268E | E | Foul Bay Rd | Chaucer St | Byron St | 82.9 | PCC | Cr_Gtt | N | 522 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_269E | SDWK_269E | E | Foul Bay Rd | Byron St | Leighton Rd | 25.4 | PCC | Cr_Gtt | N | 590 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_270E | SDWK_270E | E | Foul Bay Rd | Leighton Rd | Milton St | 58.4 | PCC | Cr_Gtt | N | 564 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_271E | SDWK_271E | E | Foul Bay Rd | Milton St | Meadow Pl | 74.2 | PCC | Cr_Gtt | N | 333 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 24% |
| Good | CRB_272E | SDWK_272E | E | Foul Bay Rd | Meadow Pl | Bourchier St | 42.5 | PCC | Cr_Gtt | N | 443 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 14% |
| Good | CRB_273E | SDWK_273E | E | Foul Bay Rd | Bourchier St | Goldsmith St | 29.2 | PCC | Cr_Gtt | N | 563 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_274E | SDWK_274E | E | Foul Bay Rd | Goldsmith St | Cadboro Bay Rd | 176.2 | PCC | Cr_Gtt | N | 60 | N | 54.0 | 0.0 | \$31,320 | \$0 | \$31,320 | 54 | 31% |
| Good | CRB_275E | SDWK_275E | E | Foul Bay Rd | Cadboro Bay Rd | Haultain St | 380.7 | PCC | Cr_Gtt | N | 415 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% |
| Fair | CRB_276E | SDWK_276E | E | Foul Bay Rd | Haultain St | Carrick St | 88.8 | PCC | Cr_Gtt | N | 664 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_277E | SDWK_277E | E | Foul Bay Rd | Carrick St | Kings Rd | 92.6 | PCC | Cr_Gtt | N | 574 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_278E | SDWK_278E | E | Foul Bay Rd | Kings Rd | Newton St | 84.3 | PCC | Cr_Gtt | N | 612 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_279E | SDWK_279E | E | Foul Bay Rd | Newton St | Carnarvon St | 87.4 | PCC | Cr_Gtt | N | 284 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 38% |
| Good | CRB_280E | SDWK_280E | E | Foul Bay Rd | Carnarvon St | Townley St | 83.8 | PCC | Cr_Gtt | N | 43 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 32% |
| Good | CRB_281E | SDWK_281E | E | Foul Bay Rd | Townley St | Allenby St | 84.4 | PCC | Cr_Gtt | N | 271 | N | 39.0 | 0.0 | \$22,620 | \$0 | \$22,620 | 39 | 46% |

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent |
|----------------|------------|-------------|-------|---------------------|-----------------------------------|-----------------------------------|-------|-----|--------|----------|------|-------|------|---------|------------------|---------------|----------|---------|-------------------|----------------|
| | | | Face | Street | From | To | | | | | | | | | PCC Length (m) | AC Length (m) | PCC Cost | AC Cost | | |
| Good | CRB_282E | SDWK_282E | E | Foul Bay Rd | Allenby St | Neil St | 112.5 | PCC | Cr_Gtt | N | 15 | N | 36.0 | 0.0 | \$20,880 | \$0 | \$20,880 | 36 | 32% | |
| Fair | CRB_283E | SDWK_283E | E | Foul Bay Rd | Neil St | Middowne Rd | 257.5 | PCC | Cr_Gtt | N | 421 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% | |
| Good | CRB_284E | SDWK_284E | E | Foul Bay Rd | Middowne Rd | Lansdowne Rd | 181.0 | PCC | Cr_Gtt | N | 424 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% | |
| Fair | CRB_285E | SDWK_285E | E | Foul Bay Rd | Lansdowne Rd | University Woods | 474.3 | PCC | Mntbl | N | 534 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_285W | SDWK_285W | W | Foul Bay Rd | Lansdowne Rd | University Woods | 483.1 | PCC | Mntbl | N | 530 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_286E | SDWK_286E | E | Foul Bay Rd | University Woods | Gibbs Rd | 65.5 | PCC | Mntbl | N | 641 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_286W | SDWK_286W | W | Foul Bay Rd | University Woods | Gibbs Rd | 58.4 | PCC | Mntbl | N | 586 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_287E | SDWK_287E | E | Foul Bay Rd | Gibbs Rd | Henderson Rd | 117.9 | PCC | Cr_Gtt | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% | |
| Fair | CRB_287W | SDWK_287W | W | Foul Bay Rd | Gibbs Rd | Henderson Rd | 73.2 | PCC | Mntbl | N | 149 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_288EW | SDWK_288EW | EW | Frederick Norris Pl | Northwest End | Frederick Norris Rd / Redwood Ave | 296.9 | PCC | Mntbl | N | 386 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 3% | |
| Fair | CRB_289N | SDWK_289N | N | Frederick Norris Rd | Frederick Norris Pl / Redwood Ave | Kelsey Pl | 61.1 | PCC | Mntbl | N | 587 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_289S | SDWK_289S | S | Frederick Norris Rd | Frederick Norris Pl / Redwood Ave | Kelsey Pl | 50.8 | PCC | Mntbl | N | 444 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 12% | |
| Fair | CRB_290N | SDWK_290N | N | Frederick Norris Rd | Kelsey Pl | Cardiff Pl | 117.0 | PCC | Mntbl | N | 594 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_290S | SDWK_290S | S | Frederick Norris Rd | Kelsey Pl | Cardiff Pl | 95.2 | PCC | Mntbl | N | 120 | N | 39.0 | 0.0 | \$22,620 | \$0 | \$22,620 | 39 | 41% | |
| Fair | CRB_291N | SDWK_291N | N | Frederick Norris Rd | Cardiff Pl | Henderson Rd | 86.0 | PCC | Mntbl | N | 582 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_291S | SDWK_291S | S | Frederick Norris Rd | Cardiff Pl | Henderson Rd | 81.2 | PCC | Mntbl | N | 505 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% | |
| Fair | CRB_293E | SDWK_293E | E | Gibbs Rd | Foul Bay Rd | Northeast End | 222.0 | PCC | Mntbl | N | 360 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 5% | |
| Fair | CRB_293W | SDWK_293W | W | Gibbs Rd | Foul Bay Rd | Northeast End | 217.5 | PCC | Mntbl | N | 140 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 8% | |
| Good | CRB_294N | SDWK_294N | N | Goldsmith St | Foul Bay Rd | Bee St | 71.7 | PCC | Cr_Gtt | N | 478 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% | |
| Good | CRB_294S | SDWK_294S | S | Goldsmith St | Foul Bay Rd | Bee St | 92.9 | PCC | Mower | N | 691 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_295N | SDWK_295N | N | Goldsmith St | Bee St | East End | 114.6 | PCC | Cr_Gtt | N | 536 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_295S | SDWK_295S | S | Goldsmith St | Bee St | East End | 135.2 | PCC | Mower | N | 6 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% | |
| Fair | CRB_296EA | SDWK_296EA | E | Goodwin St | Currie Rd | Beach Dr / Windsor Rd | 44.6 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_296EB | SDWK_296EB | E | Goodwin St | Currie Rd | Beach Dr / Windsor Rd | 163.1 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% | |
| Fair | CRB_296W | SDWK_296W | W | Goodwin St | Currie Rd | Beach Dr / Windsor Rd | 152.0 | PCC | Mntbl | Y | 370 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 8% | |
| Fair | CRB_297EA | SDWK_297EA | E | Gordon Head Rd | Cedar Hill Cross Rd | Mckenzie Ave | 186.8 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_297EB | SDWK_297EB | E | Gordon Head Rd | Cedar Hill Cross Rd | Mckenzie Ave | 731.3 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_299N | SDWK_299N | N | Granite St | Foul Bay Rd | Mitchell St | 100.5 | PCC | Cr_Gtt | N | 168 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_299S | SDWK_299S | S | Granite St | Foul Bay Rd | Mitchell St | 91.0 | PCC | Cr_Gtt | N | 745 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_300N | SDWK_300N | N | Granite St | Mitchell St | Victoria Ave | 146.2 | PCC | Cr_Gtt | N | 389 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 6% | |
| Good | CRB_300S | SDWK_300S | S | Granite St | Mitchell St | Victoria Ave | 140.7 | PCC | Cr_Gtt | N | 540 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_301N_A | SDWK_301N_A | N | Granite St | Victoria Ave | Hampshire Rd | 22.4 | PCC | Cr_Gtt | N | 166 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 27% | |
| Good | CRB_301N_B | SDWK_301N_B | N | Granite St | Victoria Ave | Hampshire Rd | 106.2 | PCC | Cr_Gtt | N | 543 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_301N_C | SDWK_301N_C | N | Granite St | Victoria Ave | Hampshire Rd | 75.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_301S | SDWK_301S | S | Granite St | Victoria Ave | Hampshire Rd | 201.1 | PCC | Cr_Gtt | N | 549 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_305N | SDWK_305N | N | Hamiota St | Cadboro Bay Rd | Estevan Ave | 311.4 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% | |
| Fair | CRB_305S | SDWK_305S | S | Hamiota St | Cadboro Bay Rd | Estevan Ave | 320.7 | PCC | Mntbl | N | 788 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_306E | SDWK_306E | E | Hampshire Rd | Lafayette St | Guernsey St | 136.2 | PCC | Cr_Gtt | N | 726 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_306W | SDWK_306W | W | Hampshire Rd | Lafayette St | Guernsey St | 128.2 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% | |
| Fair | CRB_307E | SDWK_307E | E | Hampshire Rd | Guernsey St | Bartlett Ave | 90.6 | PCC | Mntbl | N | 62 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Poor | CRB_307W | SDWK_307W | W | Hampshire Rd | Guernsey St | Bartlett Ave | 86.1 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | |
| Fair | CRB_308E | SDWK_308E | E | Hampshire Rd | Bartlett Ave | Central Ave | 78.9 | PCC | Mntbl | Y | 737 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Poor | CRB_308W | SDWK_308W | W | Hampshire Rd | Bartlett Ave | Central Ave | 75.8 | PCC | Mntbl | Y | 0 | Y | 75.0 | 0.0 | \$43,500 | \$0 | \$43,500 | 75 | 99% | |
| Fair | CRB_309E | SDWK_309E | E | Hampshire Rd | Central Ave | Beaverbrooke St | 140.6 | PCC | Mntbl | Y | 69 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% | |
| Fair | CRB_309W | SDWK_309W | W | Hampshire Rd | Central Ave | Beaverbrooke St | 134.5 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% | |
| Good | CRB_310E | SDWK_310E | E | Hampshire Rd | Beaverbrooke St | Smythe St | 99.3 | PCC | Mntbl | Y | 508 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | |
| Good | CRB_310W | SDWK_310W | W | Hampshire Rd | Beaverbrooke St | Smythe St | 92.0 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_311E | SDWK_311E | E | Hampshire Rd | Smythe St | Pentland Rd | 53.6 | PCC | Cr_Gtt | Y | 789 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_311W | SDWK_311W | W | Hampshire Rd | Smythe St | Pentland Rd | 55.9 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_312E | SDWK_312E | E | Hampshire Rd | Pentland Rd | McNeill Ave | 99.6 | PCC | Cr_Gtt | Y | 506 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | |
| Fair | CRB_312W | SDWK_312W | W | Hampshire Rd | Pentland Rd | McNeill Ave | 94.8 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% | |
| Fair | CRB_313E | SDWK_313E | E | Hampshire Rd | McNeill Ave | Windsor Rd | 185.1 | PCC | Mntbl | N | 52 | N | 36.0 | 0.0 | \$20,880 | \$0 | \$20,880 | 36 | 19% | |
| Fair | CRB_313W | SDWK_313W | W | Hampshire Rd | McNeill Ave | Windsor Rd | 186.6 | PCC | Mntbl | N | 310 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 13% | |
| Good | CRB_314E | SDWK_314E | E | Hampshire Rd | Windsor Rd | Brighton Ave | 233.8 | PCC | Cr_Gtt | Y | 33 | N | 39.0 | 0.0 | \$22,620 | \$0 | \$22,620 | 39 | 17% | |
| Good | CRB_314W | SDWK_314W | W | Hampshire Rd | Windsor Rd | Brighton Ave | 237.4 | PCC | Mntbl | Y | 56 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 8% | |
| Fair | CRB_315E | SDWK_315E | E | Hampshire Rd | Brighton Ave | Hampshire Ter | 92.0 | PCC | Mntbl | Y | 401 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 10% | |
| Fair | CRB_315W | SDWK_315W | W | Hampshire Rd | Brighton Ave | Hampshire Ter | 91.2 | PCC | Mntbl | Y | 316 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 26% | |
| Fair | CRB_316E | SDWK_316E | E | Hampshire Rd | Hampshire Ter | Granite St | 38.6 | PCC | Mntbl | Y | 474 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 8% | |
| Good | CRB_316W | SDWK_316W | W | Hampshire Rd | Hampshire Ter | Granite St | 35.5 | PCC | Cr_Gtt | Y | 448 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 17% | |
| Good | CRB_317E | SDWK_317E | E | Hampshire Rd | Granite St | Oak Bay Ave | 116.1 | PCC | Cr_Gtt | Y | 298 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 23% | |
| Good | CRB_317W | SDWK_317W | W | Hampshire Rd | Granite St | Oak Bay Ave | 110.8 | PCC | Cr_Gtt | Y | 334 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 16% | |
| Good | CRB_318E_A | SDWK_318E_A | E | Hampshire Rd | Oak Bay Ave | Cubbon Dr | 37.7 | PCC | Mntbl | N | 642 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_318E_B | SDWK_318E_B | E | Hampshire Rd | Oak Bay Ave | Cubbon Dr | 278.8 | PCC | Mntbl | Y | 591 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_318W_A | SDWK_318W_A | W | Hampshire Rd | Oak Bay Ave | Cubbon Dr | 42.9 | PCC | Mntbl | N | 790 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_318W_B | SDWK_318W_B | W | Hampshire Rd | Oak Bay Ave | Cubbon Dr | 275.1 | PCC | Mntbl | N | 346 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 5% | |
| Good | CRB_319E | SDWK_319E | E | Hampshire Rd | Cubbon Dr | Cranmore Rd | 281.6 | PCC | Mntbl | Y | 571 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_319W | SDWK_319W | W | Hampshire Rd | Cubbon Dr | Cranmore Rd | 279.8 | PCC | Mntbl | Y | 527 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_320E | SDWK_320E | E | Hampshire Rd | Cranmore Rd | Bowker Ave | 222.4 | PCC | Cr_Gtt | Y | 722 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_320W | SDWK_320W | W | Hampshire Rd | Cranmore Rd | Bowker Ave | 225.0 | PCC | Cr_Gtt | Y | 757 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_321E | SDWK_321E | E | Hampshire Rd | Bowker Ave | Cavendish Ave / Musgrave St | 95.5 | PCC | Mntbl | Y | 290 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 31% | |
| Fair | CRB_321W | SDWK_321W | W | Hampshire Rd | Bowker Ave | Cavendish Ave / Musgrave St | 106.4 | PCC | Mntbl | Y | 287 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 28% | |
| Fair | CRB_324N | SDWK_324N | N | Harlow Dr | Eastdowne Rd | Cadboro Bay Rd | 174.4 | PCC | Mntbl | N | 616 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Fair | CRB_324S | SDWK_324S | S | Harlow Dr | Eastdowne Rd | Cadboro Bay Rd | 178.3 | PCC | Mntbl | N | 231 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 5% | |
| Fair | CRB_325N | SDWK_325N | N | Haultain St | Foul Bay Rd | Florence St | 99.7 | PCC | Mntbl | Y | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 6% | |
| Fair | CRB_325S | SDWK_325S | S | Haultain St | Foul Bay Rd | Florence St | 89.2 | PCC | Mower | Y | 544 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |
| Good | CRB_326N | SDWK_326N | N | Haultain St | Florence St | Epworth St | 20.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% | |

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | From | To | Length | Material | Type | Paved | | | Curb Replacement | | | Recon. Length (m) | Recon. Percent | | |
|----------------|------------|-------------|-------|-----------------|--------------------------------|-------------------------------------|--------|----------|--------|--------|----------|-------|------------------|---------------|-----------|-------------------|----------------|---------|------------|
| | | | Face | Street | | | | | | Gutter | Priority | Proj? | PCC Length (m) | AC Length (m) | PCC Cost | | | AC Cost | Total Cost |
| Fair | CRB_326S | SDWK_326S | S | Haultain St | Florence St | Epworth St | 89.4 | PCC | Mower | Y | 541 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_327N | SDWK_327N | N | Haultain St | Epworth St | Henderson Rd | 64.0 | PCC | Mntbl | Y | 537 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_327S | SDWK_327S | S | Haultain St | Epworth St | Henderson Rd | 51.9 | PCC | Mntbl | Y | 376 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 23% |
| Fair | CRB_328N | SDWK_328N | N | Haultain St | Henderson Rd | Eastdowne Rd | 157.7 | PCC | Mntbl | N | 239 | N | 0.0 | 0.0 | \$91,640 | \$0 | \$91,640 | 0 | 0% |
| Fair | CRB_328S | SDWK_328S | S | Haultain St | Henderson Rd | Eastdowne Rd | 145.6 | PCC | Mntbl | N | 366 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 8% |
| Fair | CRB_332E | SDWK_332E | E | Henderson Rd | Newton St | Carnarvon St | 93.1 | PCC | Mntbl | Y | 127 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 6% |
| Poor | CRB_332W | SDWK_332W | W | Henderson Rd | Newton St | Carnarvon St | 88.7 | PCC | Mntbl | Y | 224 | Y | 90.0 | 0.0 | \$52,200 | \$0 | \$52,200 | 90 | 100% |
| Good | CRB_333E | SDWK_333E | E | Henderson Rd | Carnarvon St | Townley St | 94.6 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_333W | SDWK_333W | W | Henderson Rd | Carnarvon St | Townley St | 83.4 | PCC | Mntbl | N | 791 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_334E | SDWK_334E | E | Henderson Rd | Townley St | Allenby St | 88.1 | PCC | Mntbl | N | 233 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% |
| Fair | CRB_334W | SDWK_334W | W | Henderson Rd | Townley St | Allenby St | 84.0 | PCC | Mntbl | N | 403 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 11% |
| Good | CRB_335E | SDWK_335E | E | Henderson Rd | Allenby St | Neil St | 83.9 | PCC | Cr_Gtt | N | 211 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_335W | SDWK_335W | W | Henderson Rd | Allenby St | Neil St | 84.2 | PCC | Mntbl | N | 193 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_336E | SDWK_336E | E | Henderson Rd | Neil St | Middowne Rd | 236.7 | PCC | Mntbl | N | 210 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_336W | SDWK_336W | W | Henderson Rd | Neil St | Middowne Rd | 238.2 | PCC | Cr_Gtt | N | 750 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_337E | SDWK_337E | E | Henderson Rd | Middowne Rd | Lansdowne Rd | 217.6 | PCC | Mntbl | N | 568 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_337W | SDWK_337W | W | Henderson Rd | Middowne Rd | Lansdowne Rd | 213.3 | PCC | Mntbl | N | 471 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_338E | SDWK_338E | E | Henderson Rd | Lansdowne Rd | Westdowne Rd | 149.0 | PCC | Mntbl | N | 416 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% |
| Fair | CRB_338W | SDWK_338W | W | Henderson Rd | Lansdowne Rd | Westdowne Rd | 143.1 | PCC | Mntbl | N | 583 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_339E | SDWK_339E | E | Henderson Rd | Westdowne Rd | Wessex Cir / Wessex Cr | 139.3 | PCC | Mntbl | N | 526 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_339W | SDWK_339W | W | Henderson Rd | Westdowne Rd | Wessex Cir / Wessex Cr | 131.8 | PCC | Mntbl | N | 404 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 7% |
| Fair | CRB_340E | SDWK_340E | E | Henderson Rd | Wessex Cir / Wessex Cr | Foul Bay Rd | 203.9 | PCC | Mntbl | N | 139 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_340W | SDWK_340W | W | Henderson Rd | Wessex Cir / Wessex Cr | Foul Bay Rd | 140.9 | PCC | Mntbl | N | 85 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_341E | SDWK_341E | E | Henderson Rd | Foul Bay Rd | Dundrum Rd | 50.8 | PCC | Mntbl | Y | 146 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_341W | SDWK_341W | W | Henderson Rd | Foul Bay Rd | Dundrum Rd | 56.6 | PCC | Mntbl | Y | 637 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_342E | SDWK_342E | E | Henderson Rd | Dundrum Rd | Kendal Ave | 184.4 | PCC | Mntbl | Y | 148 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_342W | SDWK_342W | W | Henderson Rd | Dundrum Rd | Kendal Ave | 186.1 | PCC | Mntbl | Y | 325 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 11% |
| Fair | CRB_343EA | SDWK_343EA | E | Henderson Rd | Kendal Ave | Avondale Rd | 86.9 | PCC | Mntbl | Y | 356 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 17% |
| Fair | CRB_343EB | SDWK_343EB | E | Henderson Rd | Kendal Ave | Avondale Rd | 205.6 | PCC | Mntbl | Y | 434 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_343W | SDWK_343W | W | Henderson Rd | Kendal Ave | Avondale Rd | 287.9 | PCC | Mntbl | Y | 516 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_344E | SDWK_344E | E | Henderson Rd | Avondale Rd | Arden Rd | 86.3 | PCC | Mntbl | Y | 203 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_344W | SDWK_344W | W | Henderson Rd | Avondale Rd | Arden Rd | 83.4 | PCC | Mntbl | Y | 184 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_345E | SDWK_345E | E | Henderson Rd | Arden Rd | Murdoch Cr | 123.1 | PCC | Mntbl | Y | 130 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 10% |
| Fair | CRB_345W | SDWK_345W | W | Henderson Rd | Arden Rd | Murdoch Cr | 140.2 | PCC | Mntbl | Y | 464 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_346E | SDWK_346E | E | Henderson Rd | Murdoch Cr | Murdoch Cr | 40.1 | Stone | Mntbl | N | 89 | Y | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 60% |
| Fair | CRB_346W | SDWK_346W | W | Henderson Rd | Murdoch Cr | Murdoch Cr | 55.8 | PCC | Mntbl | Y | 410 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 16% |
| Fair | CRB_347E | SDWK_347E | E | Henderson Rd | Murdoch Cr | Frederick Norris Rd | 49.0 | PCC | Mntbl | Y | 105 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 18% |
| Fair | CRB_347W | SDWK_347W | W | Henderson Rd | Murdoch Cr | Frederick Norris Rd | 52.1 | PCC | Mntbl | Y | 470 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 6% |
| Fair | CRB_348E | SDWK_348E | E | Henderson Rd | Frederick Norris Rd | Cedar Hill Cross Rd / University Dr | 72.2 | PCC | Mntbl | Y | 500 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_348W | SDWK_348W | W | Henderson Rd | Frederick Norris Rd | Cedar Hill Cross Rd / University Dr | 73.5 | PCC | Cr_Gtt | Y | 792 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_349E | SDWK_349E | E | Heron St | Lincoln Rd | Estevan Ave | 198.3 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_349W | SDWK_349W | W | Heron St | Lincoln Rd | Estevan Ave | 200.8 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Poor | CRB_350E | SDWK_350E | E | Heron St | Estevan Ave | Burdick Ave | 218.2 | PCC | Mntbl | Y | 0 | Y | 219.0 | 0.0 | \$127,020 | \$0 | \$127,020 | 219 | 100% |
| Poor | CRB_350W | SDWK_350W | W | Heron St | Estevan Ave | Burdick Ave | 217.8 | PCC | Mntbl | Y | 0 | Y | 219.0 | 0.0 | \$127,020 | \$0 | \$127,020 | 219 | 100% |
| Poor | CRB_351E | SDWK_351E | E | Heron St | Burdick Ave | Dorset Rd | 215.3 | PCC | Mntbl | Y | 0 | Y | 216.0 | 0.0 | \$125,280 | \$0 | \$125,280 | 216 | 100% |
| Poor | CRB_351W | SDWK_351W | W | Heron St | Burdick Ave | Dorset Rd | 215.8 | PCC | Mntbl | Y | 0 | Y | 216.0 | 0.0 | \$125,280 | \$0 | \$125,280 | 216 | 100% |
| Good | CRB_352E | SDWK_352E | E | Hewlett Pl | South End | Brighton Ave | 144.1 | PCC | Cr_Gtt | Y | 21 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% |
| Good | CRB_352W | SDWK_352W | W | Hewlett Pl | South End | Brighton Ave | 144.1 | PCC | Cr_Gtt | Y | 754 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_353S | SDWK_353S | S | Hibbens Cs | Cadboro Bay Rd | East End | 62.0 | AC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_356NS | SDWK_356NS | NS | Humber Rd | Beach Dr | East End | 327.6 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_357E | SDWK_357E | E | Inglewood Ter | Beach Dr | North End | 114.4 | AC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_362W | SDWK_362W | W | Island Rd | McNeill Ave | Currie Rd | 54.6 | AC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_363WE | SDWK_363WE | WE | Kelsey Pl | South End | Frederick Norris Rd | 182.4 | PCC | Mntbl | N | 128 | N | 57.0 | 0.0 | \$33,060 | \$0 | \$33,060 | 57 | 31% |
| Fair | CRB_364NS | SDWK_364NS | NS | Kendal Ave | West End | Plymouth Rd | 187.1 | PCC | Mntbl | Y | 385 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 5% |
| Good | CRB_365N | SDWK_365N | N | Kendal Ave | Plymouth Rd | Cardiff Pl | 173.8 | PCC | Mntbl | Y | 257 | N | 54.0 | 0.0 | \$31,320 | \$0 | \$31,320 | 54 | 31% |
| Fair | CRB_365S | SDWK_365S | S | Kendal Ave | Plymouth Rd | Cardiff Pl | 175.0 | PCC | Mntbl | Y | 141 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_366N | SDWK_366N | N | Kendal Ave | Cardiff Pl | Cardiff Pl | 35.4 | PCC | Mntbl | Y | 639 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_366S | SDWK_366S | S | Kendal Ave | Cardiff Pl | Cardiff Pl | 34.2 | PCC | Mntbl | Y | 129 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 9% |
| Fair | CRB_367N | SDWK_367N | N | Kendal Ave | Cardiff Pl | Henderson Rd | 101.0 | PCC | Mntbl | Y | 592 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_367S | SDWK_367S | S | Kendal Ave | Cardiff Pl | Henderson Rd | 107.4 | PCC | Mntbl | Y | 144 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 8% |
| Good | CRB_368N | SDWK_368N | N | King George Ter | Crescent Rd | Barkley Ter / King George Terr | 98.9 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_368S | SDWK_368S | S | King George Ter | Crescent Rd | Barkley Ter / King George Terr | 92.2 | PCC | Mntbl | N | 261 | N | 45.0 | 0.0 | \$26,100 | \$0 | \$26,100 | 45 | 49% |
| Fair | CRB_369E | SDWK_369E | E | King George Ter | Barkley Ter / King George Terr | Prince Edward Dr | 171.2 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Good | CRB_369W | SDWK_369W | W | King George Ter | Barkley Ter / King George Terr | Prince Edward Dr | 175.6 | PCC | Mntbl | N | 318 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 14% |
| Fair | CRB_370E | SDWK_370E | E | King George Ter | Prince Edward Dr | Prince Andrew Pl | 84.0 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_370W | SDWK_370W | W | King George Ter | Prince Edward Dr | Prince Andrew Pl | 80.1 | PCC | Mntbl | N | 557 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_371N | SDWK_371N | N | King George Ter | Prince Andrew Pl | Sunny Ln | 485.4 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 1% |
| Good | CRB_371S_A | SDWK_371S_A | S | King George Ter | Prince Andrew Pl | Sunny Ln | 492.0 | PCC | Cr_Gtt | N | 265 | N | 42.0 | 0.0 | \$24,360 | \$0 | \$24,360 | 42 | 9% |
| Good | CRB_371S_B | SDWK_371S_B | S | King George Ter | Prince Andrew Pl | Sunny Ln | 35.1 | PCC | Cr_Gtt | N | 793 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_371S_C | SDWK_371S_C | S | King George Ter | Prince Andrew Pl | Sunny Ln | 100.7 | PCC | Cr_Gtt | N | 556 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_372N | SDWK_372N | N | King George Ter | Sunny Ln | Beach Dr | 104.3 | PCC | Cr_Gtt | Y | 307 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 26% |
| Fair | CRB_372S | SDWK_372S | S | King George Ter | Sunny Ln | Beach Dr | 105.0 | PCC | Cr_Gtt | Y | 580 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_373N | SDWK_373N | N | Kings Rd | Foul Bay Rd | Wootton Cr | 351.4 | PCC | Mntbl | N | 456 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_373S | SDWK_373S | S | Kings Rd | Foul Bay Rd | Wootton Cr | 331.2 | PCC | Mntbl | N | 520 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_374N | SDWK_374N | N | Kinross Ave | Eastdowne Rd | Mowat St | 79.6 | PCC | Mntbl | Y | 705 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_374S | SDWK_374S | S | Kinross Ave | Eastdowne Rd | Mowat St | 84.4 | PCC | Mntbl | N | 794 | N | 0.0 | 0.0 | \$0 | | | | |

**DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY**

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | |
|----------------|-----------|-----------|--------|--------------|--------------------------|--------|----------|--------|-------|-----|---------|------------------|---------------|-----------|---------|-------------------|----------------|------------|
| | | Face | Street | | | | | | | | | PCC Length (m) | AC Length (m) | PCC Cost | AC Cost | | | Total Cost |
| Fair | CRB_375N | SDWK_375N | N | Kinross Ave | Mowat St | 214.7 | PCC | Mntbl | N | 427 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_375S | SDWK_375S | S | Kinross Ave | Mowat St | 201.6 | PCC | Mntbl | N | 439 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_378N | SDWK_378N | N | Lansdowne Rd | Foul Bay Rd | 175.1 | PCC | Cr_Gtt | Y | 595 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_378S | SDWK_378S | S | Lansdowne Rd | Foul Bay Rd | 178.7 | PCC | Mntbl | Y | 460 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_379N | SDWK_379N | N | Lansdowne Rd | Westdowne Rd | 124.5 | PCC | Cr_Gtt | Y | 197 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_379S | SDWK_379S | S | Lansdowne Rd | Westdowne Rd | 120.4 | PCC | Cr_Gtt | Y | 332 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 15% |
| Fair | CRB_380N | SDWK_380N | N | Lansdowne Rd | Henderson Rd | 135.2 | PCC | Mntbl | Y | 709 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_380S | SDWK_380S | S | Lansdowne Rd | Henderson Rd | 129.4 | PCC | Cr_Gtt | Y | 219 | Y | 129.0 | 0.0 | \$74,820 | \$0 | \$74,820 | 129 | 100% |
| Fair | CRB_381N | | N | Lansdowne Rd | Larkdowne Rd | 212.4 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_381S | SDWK_381S | S | Lansdowne Rd | Larkdowne Rd | 197.6 | PCC | Cr_Gtt | Y | 250 | N | 66.0 | 0.0 | \$38,280 | \$0 | \$38,280 | 66 | 33% |
| Fair | CRB_382N | | N | Lansdowne Rd | Eastdowne Rd | 122.2 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_382S | SDWK_382S | S | Lansdowne Rd | Eastdowne Rd | 113.4 | PCC | Cr_Gtt | Y | 545 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_383N | SDWK_383N | N | Lansdowne Rd | Cadboro Bay Rd | 189.2 | PCC | Mower | Y | 72 | Y | 189.0 | 0.0 | \$109,620 | \$0 | \$109,620 | 189 | 100% |
| Fair | CRB_384S | | S | Lansdowne Rd | Uplands Rd | 248.5 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_385N | SDWK_385N | N | Lansdowne Rd | Nottingham Rd / Weald Rd | 119.0 | PCC | Mower | Y | 177 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_385S | SDWK_385S | S | Lansdowne Rd | Nottingham Rd / Weald Rd | 122.3 | PCC | Mower | Y | 169 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_385S | SDWK_385S | S | Lansdowne Rd | Nottingham Rd / Weald Rd | 8.7 | PCC | Mower | Y | 169 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_385S | SDWK_385S | S | Lansdowne Rd | Nottingham Rd / Weald Rd | 9.2 | PCC | Mower | Y | 169 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_387N | SDWK_387N | N | Lansdowne Rd | Midland Rd | 151.1 | PCC | Mower | Y | 71 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 6% |
| Fair | CRB_387S | SDWK_387S | S | Lansdowne Rd | Midland Rd | 149.0 | PCC | Mower | Y | 70 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_388N | SDWK_388N | N | Lansdowne Rd | Ripon Rd | 210.7 | PCC | Mower | Y | 5 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Poor | CRB_388S | | S | Lansdowne Rd | Ripon Rd | 168.6 | PCC | Cr_Gtt | Y | 0 | Y | 168.0 | 0.0 | \$97,440 | \$0 | \$97,440 | 168 | 100% |
| Fair | CRB_389N | SDWK_389N | N | Lansdowne Rd | Southdowne Rd | 42.7 | PCC | Mower | Y | 192 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_389S | | S | Lansdowne Rd | Southdowne Rd | 45.6 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_390N | SDWK_390N | N | Lansdowne Rd | Norfolk Rd | 80.1 | PCC | Mower | Y | 654 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_390S | | S | Lansdowne Rd | Norfolk Rd | 58.6 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_391N | SDWK_391N | N | Lansdowne Rd | Rutland Rd | 64.6 | PCC | Mower | Y | 208 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_391S | | S | Lansdowne Rd | Rutland Rd | 172.3 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_392E | | E | Lansdowne Rd | Exeter Rd | 135.8 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_392N | SDWK_392N | N | Lansdowne Rd | Exeter Rd | 131.6 | PCC | Mower | Y | 215 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_394E | SDWK_394E | E | Larkdowne Rd | Sandowne Rd | 90.6 | PCC | Mntbl | N | 796 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_394N | SDWK_394N | N | Larkdowne Rd | Sandowne Rd | 243.3 | PCC | Mower | Y | 94 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_394W | SDWK_394W | W | Larkdowne Rd | Sandowne Rd | 96.4 | PCC | Mntbl | N | 663 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_395E | SDWK_395E | E | Larkdowne Rd | Middowne Rd | 275.0 | PCC | Mntbl | N | 269 | N | 39.0 | 0.0 | \$22,620 | \$0 | \$22,620 | 39 | 14% |
| Good | CRB_395W | SDWK_395W | W | Larkdowne Rd | Middowne Rd | 272.1 | PCC | Mntbl | N | 167 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_396E | | E | Lincoln Rd | Heron St | 300.4 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_396W | | W | Lincoln Rd | Heron St | 278.2 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_397E | | E | Lincoln Rd | Estevan Ave | 223.6 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_397W | | W | Lincoln Rd | Estevan Ave | 218.7 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Poor | CRB_398EA | | E | Lincoln Rd | Burdick Ave | 23.6 | PCC | Mntbl | Y | 0 | Y | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 100% |
| Poor | CRB_398EB | | E | Lincoln Rd | Burdick Ave | 183.0 | PCC | Mntbl | Y | 0 | Y | 183.0 | 0.0 | \$106,140 | \$0 | \$106,140 | 183 | 100% |
| Poor | CRB_398W | | W | Lincoln Rd | Burdick Ave | 214.1 | PCC | Mntbl | Y | 0 | Y | 213.0 | 0.0 | \$123,540 | \$0 | \$123,540 | 213 | 99% |
| Good | CRB_400E | | E | Linkleas Ave | Central Ave | 397.5 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_400W | | W | Linkleas Ave | Central Ave | 399.5 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_401W | | W | Lorne Ter | Maquinna St | 336.4 | AC | Mntbl | Y | 0 | N | 0.0 | 6.0 | \$2,280 | \$0 | \$2,280 | 6 | 2% |
| Fair | CRB_402E | | E | Lulie St | Southwest End | 113.0 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_402W | | W | Lulie St | Southwest End | 110.0 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_403E | | E | Lulie St | Cranmore Rd | 210.3 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_403W | | W | Lulie St | Cranmore Rd | 212.5 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_406W | | W | Maquinna St | Quimper St | 85.8 | AC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_407N | | N | Margate Ave | St Louis St | 170.4 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_407S | | S | Margate Ave | St Louis St | 170.2 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_408N | | N | Margate Ave | Newport Ave | 86.5 | AC | Mntbl | Y | 0 | Y | 0.0 | 86.0 | \$32,680 | \$0 | \$32,680 | 86 | 99% |
| Poor | CRB_408S | | S | Margate Ave | Newport Ave | 36.1 | AC | Mntbl | Y | 0 | Y | 0.0 | 36.0 | \$13,680 | \$0 | \$13,680 | 36 | 100% |
| Poor | CRB_408S | SDWK_408S | S | Margate Ave | Newport Ave | 118.7 | PCC | Cr_Gtt | Y | 220 | Y | 120.0 | 0.0 | \$69,600 | \$0 | \$69,600 | 120 | 100% |
| Fair | CRB_409NA | | N | Margate Ave | Deal St | 36.3 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_409NB | | N | Margate Ave | Deal St | 42.2 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% |
| Poor | CRB_409S | SDWK_409S | S | Margate Ave | Deal St | 87.3 | PCC | Cr_Gtt | Y | 226 | Y | 87.0 | 0.0 | \$50,460 | \$0 | \$50,460 | 87 | 100% |
| Good | CRB_412N | | N | Mayhew St | Thompson Ave | 89.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_412N | | N | Mayhew St | Thompson Ave | 83.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_413N | | N | Mayhew St | Dufferin Ave | 85.3 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_413N | | N | Mayhew St | Dufferin Ave | 82.5 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Poor | CRB_414N | | N | Mayhew St | Burdick Ave | 87.8 | PCC | Mntbl | Y | 0 | Y | 87.0 | 0.0 | \$50,460 | \$0 | \$50,460 | 87 | 99% |
| Poor | CRB_414S | | S | Mayhew St | Burdick Ave | 86.1 | PCC | Mntbl | Y | 0 | Y | 87.0 | 0.0 | \$50,460 | \$0 | \$50,460 | 87 | 100% |
| Fair | CRB_420N | SDWK_420N | N | McNeill Ave | Foul Bay Rd | 143.0 | PCC | Mntbl | N | 572 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_420S | SDWK_420S | S | McNeill Ave | Foul Bay Rd | 138.4 | PCC | Mntbl | Y | 204 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_421N | SDWK_421N | N | McNeill Ave | Runnymede Pl | 104.4 | PCC | Mntbl | Y | 512 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_421S | SDWK_421S | S | McNeill Ave | Runnymede Pl | 98.2 | PCC | Mntbl | Y | 113 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 24% |
| Fair | CRB_422N | SDWK_422N | N | McNeill Ave | Falkland Rd | 98.0 | PCC | Mntbl | Y | 653 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_422S | SDWK_422S | S | McNeill Ave | Falkland Rd | 111.9 | PCC | Mntbl | N | 626 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_423N | SDWK_423N | N | McNeill Ave | Victoria Ave | 36.9 | PCC | Mntbl | Y | 488 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 8% |
| Fair | CRB_423S | SDWK_423S | S | McNeill Ave | Victoria Ave | 30.5 | PCC | Cr_Gtt | Y | 797 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_424N | SDWK_424N | N | McNeill Ave | Victoria Ave | 42.3 | PCC | Mntbl | Y | 504 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% |
| Fair | CRB_424S | SDWK_424S | S | McNeill Ave | Victoria Ave | 44.3 | PCC | Mntbl | Y | 798 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_425N | SDWK_425N | N | McNeill Ave | Roslyn Rd | 81.3 | PCC | Mntbl | Y | 336 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 22% |
| Fair | CRB_425S | SDWK_425S | S | McNeill Ave | Roslyn Rd | 85.8 | PCC | Mntbl | Y | 621 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | |
|----------------|------------|-------------|-------|-----------------------|------------------------|------------------------|--------|----------|--------|-------|-----|---------|------------------|---------------|-----------|---------|-------------------|----------------|------------|
| | | | Face | Street | | | | | | | | | PCC Length (m) | AC Length (m) | PCC Cost | AC Cost | | | Total Cost |
| Fair | CRB_426N | SDWK_426N | N | McNeill Ave | Hampshire Rd | Monterey Ave | 81.1 | PCC | Mntbl | Y | 29 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_426S | SDWK_426S | S | McNeill Ave | Hampshire Rd | Monterey Ave | 76.0 | PCC | Mntbl | Y | 440 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 8% |
| Fair | CRB_427N | SDWK_427N | N | McNeill Ave | Monterey Ave | Monterey Ave | 41.8 | PCC | Mntbl | Y | 799 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_427S | SDWK_427S | S | McNeill Ave | Monterey Ave | Monterey Ave | 40.7 | PCC | Mntbl | Y | 329 | Y | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 52% |
| Fair | CRB_428N | SDWK_428N | N | McNeill Ave | Monterey Ave | Oliver St | 82.6 | PCC | Mntbl | Y | 131 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 25% |
| Fair | CRB_428S | SDWK_428S | S | McNeill Ave | Monterey Ave | Oliver St | 85.9 | PCC | Mntbl | Y | 481 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_429N | SDWK_429N | N | McNeill Ave | Oliver St | St Patrick St | 82.3 | PCC | Mntbl | Y | 800 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_429S | SDWK_429S | S | McNeill Ave | Oliver St | St Patrick St | 89.1 | PCC | Mntbl | Y | 58 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% |
| Fair | CRB_430N | SDWK_430N | N | McNeill Ave | St Patrick St | St David St | 81.5 | PCC | Mntbl | Y | 54 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_430S | SDWK_430S | S | McNeill Ave | St Patrick St | St David St | 77.8 | PCC | Mntbl | Y | 227 | Y | 78.0 | 0.0 | \$45,240 | \$0 | \$45,240 | 78 | 100% |
| Good | CRB_431N | SDWK_431N | N | McNeill Ave | St David St | Transit Rd | 64.3 | PCC | Mntbl | Y | 19 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_431S | SDWK_431S | S | McNeill Ave | St David St | Transit Rd | 68.3 | PCC | Mntbl | Y | 229 | Y | 69.0 | 0.0 | \$40,020 | \$0 | \$40,020 | 69 | 100% |
| Good | CRB_432N | SDWK_432N | N | McNeill Ave | Transit Rd | Island Rd | 113.6 | PCC | Cr_Gtt | N | 611 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_432S | SDWK_432S | S | McNeill Ave | Transit Rd | Island Rd | 117.0 | PCC | Cr_Gtt | N | 41 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_433N | SDWK_433N | N | McNeill Ave | Island Rd | Byng St | 132.8 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_433S | SDWK_433S | S | McNeill Ave | Island Rd | Byng St | 125.9 | PCC | Cr_Gtt | Y | 2 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 10% |
| Fair | CRB_434S | SDWK_434S | S | McNeill Ave | Byng St | St Louis St | 25.6 | PCC | Cr_Gtt | Y | 449 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 23% |
| Fair | CRB_434S | SDWK_434S | S | McNeill Ave | Byng St | St Louis St | 21.0 | PCC | Cr_Gtt | Y | 449 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 29% |
| Good | CRB_435N | SDWK_435N | N | McNeill Ave | St Louis St | Linkleas Ave | 56.1 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_435S | SDWK_435S | S | McNeill Ave | St Louis St | Linkleas Ave | 58.3 | PCC | Cr_Gtt | Y | 655 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_436N | SDWK_436N | N | McNeill Ave | Linkleas Ave | Newport Ave | 112.9 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_436S | SDWK_436S | S | McNeill Ave | Linkleas Ave | Newport Ave | 115.0 | PCC | Cr_Gtt | N | 288 | N | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 26% |
| Good | CRB_437S/N | SDWK_437S/N | S/N | Meadow Pl | Foul Bay Rd | East End | 362.4 | PCC | Cr_Gtt | N | 67 | N | 36.0 | 0.0 | \$20,880 | \$0 | \$20,880 | 36 | 10% |
| Fair | CRB_438N | SDWK_438N | N | Middowne Rd | Foul Bay Rd | Westdowne Rd | 113.3 | PCC | Mntbl | N | 695 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_438S | SDWK_438S | S | Middowne Rd | Foul Bay Rd | Westdowne Rd | 110.5 | PCC | Mntbl | N | 697 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_439N | SDWK_439N | N | Middowne Rd | Westdowne Rd | Henderson Rd | 87.0 | PCC | Mntbl | N | 648 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_439S | SDWK_439S | S | Middowne Rd | Westdowne Rd | Henderson Rd | 87.0 | PCC | Cr_Gtt | N | 801 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_440N | SDWK_440N | N | Middowne Rd | Henderson Rd | Larkdowne Rd | 89.9 | PCC | Mntbl | N | 665 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_440S | SDWK_440S | S | Middowne Rd | Henderson Rd | Larkdowne Rd | 86.8 | PCC | Mntbl | N | 802 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_441N | SDWK_441N | N | Middowne Rd | Larkdowne Rd | Oakdowne Rd | 91.9 | PCC | Mntbl | Y | 199 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_441S | SDWK_441S | S | Middowne Rd | Larkdowne Rd | Oakdowne Rd | 88.7 | PCC | Mntbl | Y | 200 | Y | 90.0 | 0.0 | \$52,200 | \$0 | \$52,200 | 90 | 100% |
| Fair | CRB_442N | SDWK_442N | N | Middowne Rd | Oakdowne Rd | Ashdowne Rd | 93.1 | PCC | Mntbl | Y | 195 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_442S | SDWK_442S | S | Middowne Rd | Oakdowne Rd | Ashdowne Rd | 91.7 | PCC | Mntbl | Y | 223 | Y | 93.0 | 0.0 | \$53,940 | \$0 | \$53,940 | 93 | 100% |
| Fair | CRB_443N | SDWK_443N | N | Middowne Rd | Ashdowne Rd | Eastdowne Rd | 85.0 | PCC | Mntbl | N | 101 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_443S | SDWK_443S | S | Middowne Rd | Ashdowne Rd | Eastdowne Rd | 75.8 | PCC | Mntbl | N | 677 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_444E | SDWK_444E | E | Midland Rd | Dorset Rd | Northeast End | 147.1 | PCC | Mntbl | N | 115 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% |
| Good | CRB_444W | SDWK_444W | W | Midland Rd | Dorset Rd | Northeast End | 145.3 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_445E | SDWK_445E | E | Midland Rd | South End | Lansdowne Rd | 228.5 | PCC | Mntbl | Y | 390 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 4% |
| Fair | CRB_445W | SDWK_445W | W | Midland Rd | South End | Lansdowne Rd | 231.4 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_446E | SDWK_446E | E | Midland Rd | Lansdowne Rd | Midland Rd Roundabout | 258.4 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_446W | SDWK_446W | W | Midland Rd | Lansdowne Rd | Midland Rd Roundabout | 265.2 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_447E | SDWK_447E | E | Midland Rd | Midland Rd Roundabout | Beach Dr to Midland Rd | 534.6 | PCC | Mower | Y | 75 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 3% |
| Fair | CRB_447W | SDWK_447W | W | Midland Rd | Midland Rd Roundabout | Beach Dr to Midland Rd | 541.9 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_448E | SDWK_448E | E | Midland Rd | Beach Dr to Midland Rd | Beach Dr | 119.8 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_448W | SDWK_448W | W | Midland Rd | Beach Dr to Midland Rd | Beach Dr | 142.3 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_449N | SDWK_449N | N | Midland Rd Roundabout | Midland Rd | Ripon Rd 1 | 51.8 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_449S | SDWK_449S | S | Midland Rd Roundabout | Midland Rd | Ripon Rd 1 | 54.9 | PCC | Mower | Y | 83 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 5% |
| Fair | CRB_450N | SDWK_450N | N | Midland Rd Roundabout | Ripon Rd 1 | Midland Rd | 40.7 | PCC | Mower | Y | 165 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_450W | SDWK_450W | W | Midland Rd Roundabout | Ripon Rd 1 | Midland Rd | 36.0 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_451N | SDWK_451N | N | Midland Rd Roundabout | Midland Rd | Upper Ter 1 | 38.6 | PCC | Mower | Y | 609 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_451S | SDWK_451S | S | Midland Rd Roundabout | Midland Rd | Upper Ter 1 | 35.9 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_452E | SDWK_452E | E | Midland Rd Roundabout | Upper Ter 1 | Southwest End | 25.8 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_452S | SDWK_452S | S | Midland Rd Roundabout | Upper Ter 1 | Southwest End | 23.7 | PCC | Mower | Y | 93 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_453N | SDWK_453N | N | Milton St | Foul Bay Rd | Elgin Rd | 238.4 | PCC | Mower | N | 455 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_453S | SDWK_453S_A | S | Milton St | Foul Bay Rd | Elgin Rd | 238.4 | PCC | Mower | N | 363 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 5% |
| Good | CRB_454E | SDWK_454E | E | Mitchell St | Brighton Ave | Granite St | 104.4 | PCC | Cr_Gtt | Y | 322 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 20% |
| Good | CRB_454W | SDWK_454W | W | Mitchell St | Brighton Ave | Granite St | 103.7 | PCC | Cr_Gtt | Y | 466 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_455E | SDWK_455E | E | Mitchell St | Granite St | Oak Bay Ave | 133.4 | PCC | Cr_Gtt | Y | 548 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_455W | SDWK_455W | W | Mitchell St | Granite St | Oak Bay Ave | 132.8 | PCC | Cr_Gtt | Y | 20 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_457E | SDWK_457E | E | Monteith St | Cranmore Rd | Bowker Ave | 206.1 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_457W | SDWK_457W | W | Monteith St | Cranmore Rd | Bowker Ave | 207.3 | PCC | Cr_Gtt | N | 562 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_458E | SDWK_458E | E | Monterey Ave | Beach Dr | Lafayette St | 109.1 | PCC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_458W | SDWK_458W | W | Monterey Ave | Beach Dr | Lafayette St | 109.0 | PCC | Cr_Gtt | Y | 515 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_459E | SDWK_459E | E | Monterey Ave | Lafayette St | Central Ave | 295.9 | PCC | Mntbl | Y | 0 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 3% |
| Fair | CRB_459W | SDWK_459W | W | Monterey Ave | Lafayette St | Central Ave | 288.7 | PCC | Mntbl | Y | 514 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_460E | SDWK_460E | E | Monterey Ave | Central Ave | Tinto St | 313.2 | PCC | Mntbl | Y | 24 | N | 93.0 | 0.0 | \$53,940 | \$0 | \$53,940 | 93 | 30% |
| Poor | CRB_460W | SDWK_460W | W | Monterey Ave | Central Ave | Tinto St | 311.1 | PCC | Mntbl | Y | 0 | Y | 312.0 | 0.0 | \$180,960 | \$0 | \$180,960 | 312 | 100% |
| Fair | CRB_461E | SDWK_461E | E | Monterey Ave | Tinto St | McNeill Ave | 75.3 | PCC | Mntbl | Y | 608 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_461W | SDWK_461W | W | Monterey Ave | Tinto St | McNeill Ave | 78.7 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_462E | SDWK_462E | E | Monterey Ave | McNeill Ave | Windsor Rd | 184.4 | PCC | Cr_Gtt | N | 285 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 18% |
| Good | CRB_462W | SDWK_462W | W | Monterey Ave | McNeill Ave | Windsor Rd | 191.3 | PCC | Cr_Gtt | N | 502 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_463E | SDWK_463E | E | Monterey Ave | Windsor Rd | Brighton Ave | 235.5 | PCC | Cr_Gtt | N | 412 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 4% |
| Good | CRB_463W | SDWK_463W | W | Monterey Ave | Windsor Rd | Brighton Ave | 241.4 | PCC | Cr_Gtt | N | 510 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_464E | SDWK_464E | E | Monterey Ave | Brighton Ave | Hampshire Ter | 83.0 | PCC | Cr_Gtt | N | 589 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_464W | SDWK_464W | W | Monterey Ave | Brighton Ave | Hampshire Ter | 87.3 | PCC | Cr_Gtt | N | 566 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_465E | SDWK_465E | E | Monterey Ave | Hampshire Ter | Oak Bay Ave | 152.3 | PCC | Cr_Gtt | N | 622 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |

**DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY**

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | |
|----------------|------------|-------------|--------|---------------|------------------------------|--------|----------|--------|-------|-----|---------|------------------|----------|-----------|----------------|-------------------|----------------|---------------|
| | | Face | Street | | | | | | | | | Gutter | Priority | Proj? | PCC Length (m) | | | AC Length (m) |
| Good | CRB_465W | SDWK_465W | W | Monterey Ave | Hampshire Ter | 153.9 | PCC | Cr_Gtt | N | 327 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 14% |
| Good | CRB_466E | SDWK_466E | E | Monterey Ave | Oak Bay Ave | 224.7 | PCC | Cr_Gtt | N | 31 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Good | CRB_466W | SDWK_466W | W | Monterey Ave | Oak Bay Ave | 229.5 | PCC | Cr_Gtt | N | 431 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Good | CRB_467E | SDWK_467E | E | Monterey Ave | Woodlawn Cr | 96.7 | PCC | Cr_Gtt | N | 735 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_467W | SDWK_467W | W | Monterey Ave | Woodlawn Cr | 119.7 | PCC | Cr_Gtt | N | 430 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% |
| Fair | CRB_468E | | E | Mountjoy Ave | Beach Dr | 162.4 | AC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_468W | | W | Mountjoy Ave | Beach Dr | 162.3 | AC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_469E | | E | Mountjoy Ave | Central Ave | 188.8 | AC | Mntbl | Y | 0 | Y | 0.0 | 189.0 | \$71,820 | \$0 | \$71,820 | 189 | 100% |
| Poor | CRB_469W | | W | Mountjoy Ave | Central Ave | 205.2 | AC | Mntbl | Y | 0 | Y | 0.0 | 205.0 | \$77,900 | \$0 | \$77,900 | 205 | 100% |
| Fair | CRB_470E | SDWK_470E | E | Mowat St | Dalhousie St | 146.1 | PCC | Mntbl | N | 485 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_470W | SDWK_470W | W | Mowat St | Dalhousie St | 150.3 | PCC | Mntbl | N | 476 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_471E | SDWK_471E | E | Mowat St | Woodhouse Rd | 31.1 | PCC | Mntbl | N | 718 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_471W | SDWK_471W | W | Mowat St | Woodhouse Rd | 29.3 | PCC | Mntbl | N | 707 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_472E | SDWK_472E | E | Murdoch Cr | Henderson Rd | 75.7 | PCC | Mntbl | N | 235 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_472E | SDWK_472E | E | Murdoch Cr | Henderson Rd | 113.2 | PCC | Mntbl | N | 235 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_473E | SDWK_473E | E | Musgrave St | Cavendish Ave / Hampshire Rd | 88.1 | PCC | Mntbl | Y | 742 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_473W | SDWK_473W | W | Musgrave St | Cavendish Ave / Hampshire Rd | 92.7 | PCC | Mntbl | Y | 273 | N | 39.0 | 0.0 | \$22,620 | \$0 | \$22,620 | 39 | 42% |
| Fair | CRB_474E | SDWK_474E | E | Musgrave St | Dalhousie St | 253.9 | PCC | Mntbl | N | 751 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_474W | SDWK_474W | W | Musgrave St | Dalhousie St | 245.0 | PCC | Mntbl | N | 57 | N | 78.0 | 0.0 | \$45,240 | \$0 | \$45,240 | 78 | 32% |
| Fair | CRB_475E | SDWK_475E | E | Musgrave St | Tod Rd | 56.5 | PCC | Mntbl | Y | 804 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_475W | SDWK_475W | W | Musgrave St | Tod Rd | 56.2 | PCC | Cr_Gtt | Y | 286 | Y | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 59% |
| Fair | CRB_477E | SDWK_477E | E | Musgrave St | Thompson Ave | 127.8 | PCC | Mntbl | N | 619 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_477W | SDWK_477W | W | Musgrave St | Thompson Ave | 102.5 | PCC | Mntbl | N | 547 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_478E | SDWK_478E | E | Musgrave St | Dufferin Ave | 96.4 | PCC | Mntbl | N | 805 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_478W | SDWK_478W | W | Musgrave St | Dufferin Ave | 95.4 | PCC | Mntbl | N | 652 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_479E | SDWK_479E | E | Musgrave St | Burdick Ave | 111.5 | PCC | Mntbl | Y | 116 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_479W | SDWK_479W | W | Musgrave St | Burdick Ave | 97.0 | PCC | Mntbl | N | 98 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_480E | SDWK_480E | E | Musgrave St | Dewdney Ave | 102.6 | PCC | Mntbl | N | 234 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_480W | SDWK_480W | W | Musgrave St | Dewdney Ave | 92.6 | PCC | Mntbl | N | 613 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_481N | SDWK_481N | N | Neil St | Foul Bay Rd | 209.4 | PCC | Mntbl | N | 417 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_481S | SDWK_481S | S | Neil St | Foul Bay Rd | 206.3 | PCC | Mntbl | N | 323 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 10% |
| Good | CRB_482N | SDWK_482N | N | Neil St | Henderson Rd | 309.3 | PCC | Mntbl | Y | 73 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_482S | SDWK_482S | S | Neil St | Henderson Rd | 301.6 | PCC | Mntbl | Y | 420 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% |
| Fair | CRB_483N | SDWK_483N | N | Neil St | Eastdowne Rd | 137.4 | PCC | Mntbl | N | 670 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_483S | SDWK_483S | S | Neil St | Eastdowne Rd | 144.5 | PCC | Mntbl | N | 669 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_484E | SDWK_484E | E | Newport Ave | Beach Dr | 279.0 | PCC | Cr_Gtt | Y | 216 | Y | 279.0 | 0.0 | \$161,820 | \$0 | \$161,820 | 279 | 100% |
| Poor | CRB_484W | SDWK_484W | W | Newport Ave | Beach Dr | 279.8 | PCC | Cr_Gtt | Y | 79 | Y | 279.0 | 0.0 | \$161,820 | \$0 | \$161,820 | 279 | 100% |
| Fair | CRB_485E | SDWK_485E | E | Newport Ave | Island Rd | 308.8 | PCC | Cr_Gtt | Y | 108 | Y | 309.0 | 0.0 | \$179,220 | \$0 | \$179,220 | 309 | 100% |
| Fair | CRB_485W | SDWK_485W | W | Newport Ave | Island Rd | 305.3 | PCC | Cr_Gtt | Y | 36 | N | 105.0 | 0.0 | \$60,900 | \$0 | \$60,900 | 105 | 34% |
| Fair | CRB_486E | SDWK_486E | E | Newport Ave | Central Ave | 356.7 | PCC | Cr_Gtt | Y | 74 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% |
| Fair | CRB_486W | SDWK_486W | W | Newport Ave | Central Ave | 360.5 | PCC | Cr_Gtt | Y | 40 | N | 51.0 | 0.0 | \$29,580 | \$0 | \$29,580 | 51 | 14% |
| Fair | CRB_487E | SDWK_487E | E | Newport Ave | Pattullo Pl | 48.8 | PCC | Cr_Gtt | Y | 38 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_487W | SDWK_487W | W | Newport Ave | Pattullo Pl | 56.3 | PCC | Cr_Gtt | Y | 350 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 27% |
| Good | CRB_488E | SDWK_488E | E | Newport Ave | McNeill Ave | 39.8 | PCC | Cr_Gtt | Y | 699 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_488W | SDWK_488W | W | Newport Ave | McNeill Ave | 39.1 | PCC | Cr_Gtt | Y | 554 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_489E | SDWK_489E | E | Newport Ave | Margate Ave | 162.0 | PCC | Mower | Y | 110 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 13% |
| Good | CRB_489W | SDWK_489W | W | Newport Ave | Margate Ave | 152.8 | PCC | Mower | Y | 109 | N | 45.0 | 0.0 | \$26,100 | \$0 | \$26,100 | 45 | 29% |
| Fair | CRB_490E | SDWK_490E | E | Newport Ave | Orchard Ave | 82.0 | PCC | Mower | Y | 482 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_490W | SDWK_490W | W | Newport Ave | Orchard Ave | 85.2 | PCC | Mower | Y | 314 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 28% |
| Fair | CRB_491E | SDWK_491E | E | Newport Ave | Currie Rd | 149.8 | PCC | Cr_Gtt | Y | 23 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 6% |
| Fair | CRB_491W | SDWK_491W | W | Newport Ave | Currie Rd | 149.1 | PCC | Cr_Gtt | Y | 102 | Y | 150.0 | 0.0 | \$87,000 | \$0 | \$87,000 | 150 | 100% |
| Good | CRB_492E | SDWK_492E | E | Newport Ave | Windsor Rd | 195.2 | PCC | Cr_Gtt | N | 344 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 8% |
| Fair | CRB_492W | SDWK_492W | W | Newport Ave | Windsor Rd | 180.0 | PCC | Cr_Gtt | N | 150 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 15% |
| Good | CRB_493E | SDWK_493E | E | Newport Ave | Transit Rd | 77.6 | PCC | Cr_Gtt | N | 507 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_493W | SDWK_493W | W | Newport Ave | Transit Rd | 87.2 | PCC | Cr_Gtt | N | 319 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 28% |
| Good | CRB_494E | SDWK_494E | E | Newport Ave | Beresford St | 121.6 | PCC | Cr_Gtt | N | 49 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_494W | SDWK_494W | W | Newport Ave | Beresford St | 113.9 | PCC | Cr_Gtt | N | 126 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 11% |
| Fair | CRB_495N | SDWK_495N | N | Newton St | Foul Bay Rd | 230.1 | PCC | Mntbl | N | 179 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_495S | SDWK_495S | S | Newton St | Foul Bay Rd | 239.7 | PCC | Mntbl | N | 207 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_496N_A | SDWK_496N_A | N | Newton St | Henderson Rd | 121.4 | PCC | Mntbl | N | 338 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 15% |
| Fair | CRB_496N_B | SDWK_496N_B | N | Newton St | Henderson Rd | 102.2 | PCC | Mntbl | N | 313 | N | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 23% |
| Fair | CRB_496S | SDWK_496S | S | Newton St | Henderson Rd | 219.4 | PCC | Mntbl | N | 96 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_498E | | E | Norfolk Rd | Lansdowne Rd | 651.7 | PCC | Mntbl | N | 0 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 1% |
| Fair | CRB_498W | SDWK_498W | W | Norfolk Rd | Lansdowne Rd | 624.5 | PCC | Mntbl | N | 132 | N | 177.0 | 0.0 | \$102,660 | \$0 | \$102,660 | 177 | 28% |
| Fair | CRB_499N | SDWK_499N | N | Nottingham Rd | Thompson Ave | 91.0 | PCC | Cr_Gtt | N | 374 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 13% |
| Fair | CRB_499S | SDWK_499S | S | Nottingham Rd | Thompson Ave | 91.2 | PCC | Mntbl | N | 561 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_500N | | N | Nottingham Rd | Dufferin Ave | 90.2 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_500S | SDWK_500S | S | Nottingham Rd | Dufferin Ave | 88.7 | PCC | Mntbl | Y | 459 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_501N | | N | Nottingham Rd | Burdick Ave | 88.5 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_501S | SDWK_501S | S | Nottingham Rd | Burdick Ave | 87.3 | PCC | Mntbl | N | 180 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_502N | | N | Nottingham Rd | Dewdney Ave | 92.8 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_502S | SDWK_502S | S | Nottingham Rd | Dewdney Ave | 86.9 | PCC | Mntbl | N | 650 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_503N | | N | Nottingham Rd | Dorset Rd | 90.9 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_503S | SDWK_503S | S | Nottingham Rd | Dorset Rd | 86.7 | PCC | Mntbl | Y | 173 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_504N | | N | Nottingham Rd | Dover Rd | 153.3 | PCC | Mntbl | N | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% |
| Fair | CRB_504S | SDWK_504S | S | Nottingham Rd | Dover Rd | 142.0 | PCC | Mntbl | Y | 428 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% |

DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | |
|----------------|------------|-------------|-------|----------------|----------------|-------|--------|----------|------|-------|----|---------|------------------|---------------|----------|---------|-------------------|----------------|------------|
| | | | Face | Street | | | | | | | | | PCC Length (m) | AC Length (m) | PCC Cost | AC Cost | | | Total Cost |
| Fair | CRB_505S | SDWK_505S | E | Nottingham Rd | Devon Rd | 45.7 | PCC | Mntbl | N | 106 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% |
| Fair | CRB_505W | | W | Nottingham Rd | Devon Rd | 45.4 | PCC | Mntbl | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_506N | SDWK_506N | N | Oak Bay Ave | Foul Bay Rd | 140.2 | PCC | Cr_Gtt | N | 22 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_506S | SDWK_506S | S | Oak Bay Ave | Foul Bay Rd | 135.5 | PCC | Cr_Gtt | N | 309 | N | | 24.0 | 0.0 | \$13,920 | \$0 | \$13,920 | 24 | 18% |
| Good | CRB_507N | SDWK_507N | N | Oak Bay Ave | Mitchell St | 97.9 | PCC | Cr_Gtt | N | 747 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_507S | SDWK_507S | S | Oak Bay Ave | Mitchell St | 98.3 | PCC | Cr_Gtt | N | 17 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_508N | SDWK_508N | N | Oak Bay Ave | Elgin Rd | 80.7 | PCC | Cr_Gtt | N | 584 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_508S | SDWK_508S | S | Oak Bay Ave | Elgin Rd | 97.5 | PCC | Cr_Gtt | N | 499 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_509N | SDWK_509N | N | Oak Bay Ave | Yale St | 85.3 | PCC | Cr_Gtt | N | 533 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_509S | SDWK_509S_B | S | Oak Bay Ave | Yale St | 98.7 | PCC | Cr_Gtt | N | 340 | N | | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 18% |
| Good | CRB_510N | SDWK_510N | N | Oak Bay Ave | Wilmot Pl | 68.5 | PCC | Cr_Gtt | N | 559 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_510S | SDWK_510S | S | Oak Bay Ave | Wilmot Pl | 63.3 | PCC | Cr_Gtt | N | 489 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 5% |
| Fair | CRB_511N | SDWK_511S | N | Oak Bay Ave | Hampshire Rd | 26.2 | PCC | Cr_Gtt | Y | 706 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_511S | SDWK_511N | S | Oak Bay Ave | Hampshire Rd | 26.5 | PCC | Cr_Gtt | N | 565 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_512N | SDWK_512N | N | Oak Bay Ave | Hampshire Rd | 85.7 | PCC | Cr_Gtt | N | 581 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_512S | SDWK_512S | S | Oak Bay Ave | Hampshire Rd | 95.8 | PCC | Cr_Gtt | N | 45 | N | | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 28% |
| Good | CRB_513N | SDWK_513N | N | Oak Bay Ave | Monterey Ave | 76.4 | PCC | Cr_Gtt | N | 618 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_513S | SDWK_513S | S | Oak Bay Ave | Monterey Ave | 79.7 | PCC | Cr_Gtt | N | 686 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_515N | SDWK_515N | N | Oak Bay Ave | Oliver St | 99.1 | PCC | Cr_Gtt | Y | 112 | N | | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 12% |
| Fair | CRB_515S | SDWK_515S | S | Oak Bay Ave | Oliver St | 80.4 | PCC | Cr_Gtt | Y | 341 | N | | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 22% |
| Fair | CRB_516N | SDWK_516N | N | Oak Bay Ave | St Patrick St | 35.6 | PCC | Cr_Gtt | Y | 529 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_516S | SDWK_516S | S | Oak Bay Ave | St Patrick St | 37.0 | PCC | Cr_Gtt | Y | 604 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_517N | SDWK_517N | N | Oak Bay Ave | York Pl | 29.0 | PCC | Cr_Gtt | Y | 553 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_517S | SDWK_517S | S | Oak Bay Ave | York Pl | 53.3 | PCC | Cr_Gtt | Y | 682 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_5001N | SDWK_5001N | N | Oak Bay Marina | Beach Dr | 131.9 | PCC | Cr_Gtt | Y | 339 | N | | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 14% |
| Good | CRB_5004S | SDWK_5004S | S | Oak Bay Marina | Beach Dr | 392.9 | PCC | Mower | Y | 546 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_520E | SDWK_520E | E | Oakdowne Rd | Middowne Rd | 97.1 | PCC | Mntbl | N | 95 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_520W | SDWK_520W | W | Oakdowne Rd | Middowne Rd | 87.1 | PCC | Mntbl | N | 194 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_521E | SDWK_521E | E | Oakdowne Rd | Middowne Rd | 267.6 | PCC | Mntbl | N | 523 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_521W | SDWK_521W | W | Oakdowne Rd | Middowne Rd | 290.5 | PCC | Mntbl | N | 629 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_522E | SDWK_522E | E | Oliver St | Beach Dr | 367.0 | PCC | Cr_Gtt | Y | 248 | N | | 84.0 | 0.0 | \$48,720 | \$0 | \$48,720 | 84 | 23% |
| Fair | CRB_522W | SDWK_522W | W | Oliver St | Beach Dr | 367.7 | PCC | Cr_Gtt | Y | 240 | N | | 120.0 | 0.0 | \$69,600 | \$0 | \$69,600 | 120 | 33% |
| Fair | CRB_523E | SDWK_523E | E | Oliver St | Central Ave | 310.7 | PCC | Cr_Gtt | Y | 253 | N | | 57.0 | 0.0 | \$33,060 | \$0 | \$33,060 | 57 | 18% |
| Fair | CRB_523W | SDWK_523W | W | Oliver St | Central Ave | 314.7 | PCC | Cr_Gtt | Y | 245 | N | | 96.0 | 0.0 | \$55,680 | \$0 | \$55,680 | 96 | 31% |
| Fair | CRB_524E | SDWK_524E | E | Oliver St | Tinto St | 82.2 | PCC | Cr_Gtt | N | 283 | N | | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 40% |
| Fair | CRB_524W | SDWK_524W | W | Oliver St | Tinto St | 81.5 | PCC | Cr_Gtt | N | 303 | N | | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 33% |
| Good | CRB_525E | SDWK_525E | E | Oliver St | McNeill Ave | 185.4 | PCC | Cr_Gtt | Y | 756 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_525W | SDWK_525W | W | Oliver St | McNeill Ave | 185.8 | PCC | Cr_Gtt | Y | 597 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_526E | SDWK_526E | E | Oliver St | Windsor Rd | 238.6 | PCC | Cr_Gtt | Y | 696 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_526W | SDWK_526W | W | Oliver St | Windsor Rd | 233.4 | PCC | Cr_Gtt | Y | 758 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_527E | SDWK_527E | E | Oliver St | Brighton Ave | 238.8 | PCC | Cr_Gtt | N | 550 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_527W | SDWK_527W | W | Oliver St | Brighton Ave | 235.6 | PCC | Cr_Gtt | N | 731 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_529N | | N | Orchard Ave | Newport Ave | 90.2 | PCC | Mntbl | Y | 0 | Y | | 90.0 | 0.0 | \$52,200 | \$0 | \$52,200 | 90 | 100% |
| Poor | CRB_529S | | S | Orchard Ave | Newport Ave | 91.9 | PCC | Mntbl | Y | 0 | Y | | 93.0 | 0.0 | \$53,940 | \$0 | \$53,940 | 93 | 100% |
| Good | CRB_530N | | N | Orchard Ave | Deal St | 98.9 | PCC | Mntbl | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_530S | SDWK_530S | S | Orchard Ave | Deal St | 59.0 | PCC | Cr_Gtt | Y | 614 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_530S_A | | S | Orchard Ave | Deal St | 43.8 | PCC | Mntbl | Y | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% |
| Fair | CRB_531N | SDWK_531N | N | Pacific Ave | Eastdowne Rd | 175.5 | PCC | Mntbl | N | 330 | N | | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 10% |
| Fair | CRB_531S | SDWK_531S | S | Pacific Ave | Eastdowne Rd | 173.8 | PCC | Mntbl | N | 277 | N | | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 19% |
| Fair | CRB_532N | SDWK_532N | N | Pacific Ave | Cadboro Bay Rd | 83.5 | PCC | Mntbl | N | 570 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_532S | SDWK_532S | S | Pacific Ave | Cadboro Bay Rd | 88.3 | PCC | Mntbl | N | 425 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% |
| Fair | CRB_533N | SDWK_533N | N | Pacific Ave | Topp Ave | 122.3 | PCC | Mntbl | N | 486 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_533S | SDWK_533S | S | Pacific Ave | Topp Ave | 135.7 | PCC | Mntbl | N | 352 | N | | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 11% |
| Good | CRB_535N | | N | Pattullo Pl | Newport Ave | 101.1 | PCC | Mntbl | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_535S | | S | Pattullo Pl | Newport Ave | 97.2 | PCC | Mntbl | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_536N | | N | Pelly Pl | Northwest End | 105.2 | PCC | Mntbl | N | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_536S | SDWK_536S | S | Pelly Pl | Northwest End | 106.2 | PCC | Mntbl | Y | 573 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_539E | | E | Penzance Rd | Southeast End | 14.9 | AC | Mntbl | Y | 0 | Y | | 0.0 | 15.0 | \$5,700 | \$7,600 | \$5,700 | 15 | 100% |
| Fair | CRB_541E | SDWK_541E | E | Plymouth Rd | Kendal Ave | 101.7 | PCC | Mntbl | N | 81 | Y | | 72.0 | 0.0 | \$41,760 | \$0 | \$41,760 | 72 | 71% |
| Fair | CRB_541W | SDWK_541W | W | Plymouth Rd | Kendal Ave | 104.1 | PCC | Mntbl | N | 151 | N | | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 12% |
| Fair | CRB_542E | SDWK_542E | E | Plymouth Rd | Renfrew Rd | 115.5 | PCC | Mntbl | N | 82 | Y | | 66.0 | 0.0 | \$38,280 | \$0 | \$38,280 | 66 | 57% |
| Fair | CRB_542W | SDWK_542W | W | Plymouth Rd | Renfrew Rd | 123.5 | PCC | Mntbl | N | 348 | N | | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 12% |
| Fair | CRB_543E | SDWK_543E | E | Plymouth Rd | Avondale Rd | 208.8 | PCC | Mntbl | N | 111 | N | | 84.0 | 0.0 | \$48,720 | \$0 | \$48,720 | 84 | 40% |
| Fair | CRB_543WA | SDWK_543WA | W | Plymouth Rd | Avondale Rd | 154.8 | PCC | Mntbl | N | 158 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_543WB | SDWK_543WB | W | Plymouth Rd | Avondale Rd | 64.3 | PCC | Mntbl | N | 159 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 9% |
| Poor | CRB_548E | | E | Quimper St | Maquinna St | 20.2 | AC | Mntbl | Y | 0 | Y | | 0.0 | 20.0 | \$7,600 | \$7,600 | \$7,600 | 20 | 99% |
| Fair | CRB_548N | | N | Quimper St | Maquinna St | 23.1 | PCC | Cr_Gtt | Y | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 13% |
| Fair | CRB_550N | | N | Rattenbury Pl | West End | 49.3 | PCC | Mntbl | Y | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 6% |
| Fair | CRB_550S | SDWK_550S | S | Rattenbury Pl | West End | 55.9 | PCC | Mntbl | Y | 138 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_551N | SDWK_551N | N | Redwood Ave | Cardiff Pl | 186.5 | PCC | Mntbl | N | 249 | N | | 75.0 | 0.0 | \$43,500 | \$0 | \$43,500 | 75 | 40% |
| Fair | CRB_551S | SDWK_551S | S | Redwood Ave | Cardiff Pl | 202.4 | PCC | Mntbl | N | 242 | Y | | 111.0 | 0.0 | \$64,380 | \$0 | \$64,380 | 111 | 55% |
| Fair | CRB_552E | SDWK_552E | E | Redwood Ave | Plymouth Rd | 130.1 | PCC | Mntbl | Y | 254 | N | | 57.0 | 0.0 | \$33,060 | \$0 | \$33,060 | 57 | 44% |
| Fair | CRB_552W | SDWK_552W | W | Redwood Ave | Plymouth Rd | 107.6 | PCC | Mntbl | Y | 181 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_553E | SDWK_553E | E | Redwood Ave | Rattenbury Pl | 62.9 | PCC | Mntbl | N | 252 | Y | | 63.0 | 0.0 | \$36,540 | \$0 | \$36,540 | 63 | 100% |
| Fair | CRB_553W | SDWK_553W | W | Redwood Ave | Rattenbury Pl | 66.4 | PCC | Mower | N | 187 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_554E | SDWK_554E | E | Redwood Ave | Pelly Pl | 58.2 | PCC | Mntbl | Y | 143 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |

**DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY**

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | SW ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | |
|----------------|-----------|------------|-------|---------------|-----------------------|---|--------|----------|--------|-------|-----|---------|------------------|---------|------------|----------------|-------------------|----------------|---------------|
| | | | Face | Street | | | | | | | | | PCC Cost | AC Cost | Total Cost | PCC Length (m) | | | AC Length (m) |
| Fair | CRB_554W | SDWK_554W | W | Redwood Ave | Pelly Pl | Frederick Norris Pl / Frederick Norris Rd | 55.9 | PCC | Mntbl | Y | 155 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_555N | SDWK_555N | N | Renfrew Rd | Plymouth Rd | Cardiff Pl | 242.6 | PCC | Mntbl | N | 183 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_555S | SDWK_555S | S | Renfrew Rd | Plymouth Rd | Cardiff Pl | 239.8 | PCC | Mntbl | N | 172 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Good | CRB_559E | | E | Ripon Rd | Lansdowne Rd | Ripon Rd 1 | 327.1 | PCC | Cr_Gtt | Y | 0 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 2% |
| Fair | CRB_560E | | E | Ripon Rd | Ripon Rd 1 | Norfolk Rd | 324.6 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_560W | SDWK_560W | W | Ripon Rd | Ripon Rd 1 | Norfolk Rd | 317.8 | PCC | Mower | Y | 66 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_561E | | E | Ripon Rd | Norfolk Rd | Beach Dr | 142.4 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_561W | SDWK_561W | W | Ripon Rd | Norfolk Rd | Beach Dr | 160.7 | PCC | Mower | Y | 623 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_562E | | E | Ripon Rd | Beach Dr | Northeast End | 120.0 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_562W | SDWK_562W | W | Ripon Rd | Beach Dr | Northeast End | 116.5 | PCC | Mower | Y | 473 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_563N | SDWK_563N | N | Ripon Rd 1 | Midland Rd Roundabout | Ripon Rd | 54.9 | PCC | Mower | Y | 125 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_563S | SDWK_563S | S | Ripon Rd 1 | Midland Rd Roundabout | Ripon Rd | 55.6 | PCC | Mower | Y | 162 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_564N | SDWK_564N | N | Rosario St | St Patrick St | Transit Rd | 146.8 | PCC | Cr_Gtt | N | 176 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_564S | | S | Rosario St | St Patrick St | Transit Rd | 147.0 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_565W | | W | Roslyn Rd | McNeill Ave | Windsor Rd | 183.3 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_565W | | W | Roslyn Rd | McNeill Ave | Windsor Rd | 184.3 | PCC | Mntbl | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Good | CRB_566E | SDWK_566E | E | Roslyn Rd | Windsor Rd | North End | 224.8 | PCC | Cr_Gtt | Y | 808 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_566W | SDWK_566W | W | Roslyn Rd | Windsor Rd | North End | 221.9 | PCC | Cr_Gtt | Y | 748 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_570E | SDWK_570E | E | Rutland Rd | Lansdowne Rd | Beach Dr | 43.3 | PCC | Mower | Y | 9 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 14% |
| Fair | CRB_570N | | N | Rutland Rd | Lansdowne Rd | Beach Dr | 249.3 | PCC | Mower | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_570S | | S | Rutland Rd | Lansdowne Rd | Beach Dr | 255.8 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_571N | SDWK_571N | N | Rutland Rd | Beach Dr | East End | 95.3 | PCC | Cr_Gtt | Y | 624 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_571S | | S | Rutland Rd | Beach Dr | East End | 87.7 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_573N | SDWK_573N | N | Sandowne Rd | Larkdowne Rd | Oakdowne Rd | 86.4 | PCC | Mntbl | N | 715 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_573S | SDWK_573S | S | Sandowne Rd | Larkdowne Rd | Oakdowne Rd | 100.5 | PCC | Mntbl | N | 740 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_574N | SDWK_574N | N | Sandowne Rd | Oakdowne Rd | Ashdowne Rd | 84.4 | PCC | Mntbl | N | 713 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_574S | SDWK_574S | S | Sandowne Rd | Oakdowne Rd | Ashdowne Rd | 100.0 | PCC | Mntbl | N | 712 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_575N | SDWK_575N | N | Sandowne Rd | Ashdowne Rd | Eastdowne Rd | 41.8 | PCC | Mntbl | N | 700 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_575S | SDWK_575S | S | Sandowne Rd | Ashdowne Rd | Eastdowne Rd | 47.3 | PCC | Cr_Gtt | N | 809 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_576N | SDWK_576N | N | Satellite St | Beach Dr | Southeast End | 230.1 | PCC | Mntbl | N | 1 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_576S | | S | Satellite St | Beach Dr | Southeast End | 35.6 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_581E | | E | Southdowne Rd | Southwest End | Lansdowne Rd | 77.4 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_581W | | W | Southdowne Rd | Southwest End | Lansdowne Rd | 40.1 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_582E | SDWK_582E | E | St Ann St | Monterey Ave | Cranmore Rd | 195.3 | PCC | Cr_Gtt | N | 377 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 6% |
| Good | CRB_582W | SDWK_582W | W | St Ann St | Monterey Ave | Cranmore Rd | 219.9 | PCC | Cr_Gtt | N | 397 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 4% |
| Good | CRB_583E | SDWK_583E | E | St Ann St | Cranmore Rd | Bowker Ave | 213.6 | PCC | Cr_Gtt | N | 675 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_583W | SDWK_583W | W | St Ann St | Cranmore Rd | Bowker Ave | 216.6 | PCC | Cr_Gtt | N | 741 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_584E | SDWK_584E | E | St David St | McNeill Ave | Windsor Rd | 184.9 | PCC | Cr_Gtt | N | 620 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_584W | SDWK_584W | W | St David St | McNeill Ave | Windsor Rd | 187.5 | PCC | Cr_Gtt | N | 736 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_585E | SDWK_585E | E | St David St | Windsor Rd | Brighton Ave | 241.4 | PCC | Cr_Gtt | Y | 575 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_585W | SDWK_585W | W | St David St | Windsor Rd | Brighton Ave | 237.4 | PCC | Cr_Gtt | Y | 810 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_586E | SDWK_586E | E | St David St | Brighton Ave | Newport Ave / Oak Bay Ave | 206.4 | PCC | Cr_Gtt | Y | 811 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_586W | SDWK_586W | W | St David St | Brighton Ave | Newport Ave / Oak Bay Ave | 206.9 | PCC | Cr_Gtt | Y | 714 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_588E | | E | St Louis St | McNeill Ave | Margate Ave | 41.3 | AC | Mntbl | Y | 0 | Y | 0.0 | 41.0 | \$15,580 | \$15,580 | \$15,580 | 41 | 99% |
| Poor | CRB_588W | | W | St Louis St | McNeill Ave | Margate Ave | 44.1 | AC | Mntbl | Y | 0 | Y | 0.0 | 44.0 | \$16,720 | \$16,720 | \$16,720 | 44 | 100% |
| Fair | CRB_591E | SDWK_591E | E | St Patrick St | Beach Dr | Central Ave | 362.0 | PCC | Cr_Gtt | Y | 823 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_591W | SDWK_591W | W | St Patrick St | Beach Dr | Central Ave | 352.1 | PCC | Cr_Gtt | Y | 39 | N | 93.0 | 0.0 | \$53,940 | \$0 | \$53,940 | 93 | 26% |
| Good | CRB_592E | SDWK_592E | E | St Patrick St | Central Ave | Rosario St | 125.5 | PCC | Cr_Gtt | N | 531 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_592W | SDWK_592W | W | St Patrick St | Central Ave | Rosario St | 131.7 | PCC | Cr_Gtt | N | 68 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_593E | SDWK_593E | E | St Patrick St | Rosario St | Zela St | 74.7 | PCC | Cr_Gtt | Y | 812 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_593W | SDWK_593W | W | St Patrick St | Rosario St | Zela St | 87.3 | PCC | Cr_Gtt | Y | 59 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_594E | SDWK_594E | E | St Patrick St | Zela St | Cookman St | 79.5 | PCC | Cr_Gtt | Y | 813 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_594W | SDWK_594W | W | St Patrick St | Zela St | Cookman St | 90.0 | PCC | Cr_Gtt | Y | 765 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_595E | SDWK_595E | E | St Patrick St | Cookman St | McNeill Ave | 80.1 | PCC | Cr_Gtt | N | 739 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_595W | SDWK_595W | W | St Patrick St | Cookman St | McNeill Ave | 83.4 | PCC | Cr_Gtt | N | 764 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_596E | SDWK_596E | E | St Patrick St | McNeill Ave | Windsor Rd | 185.5 | PCC | Cr_Gtt | N | 728 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_596W | SDWK_596W | W | St Patrick St | McNeill Ave | Windsor Rd | 186.4 | PCC | Cr_Gtt | N | 493 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Good | CRB_597E | SDWK_597E | E | St Patrick St | Windsor Rd | Brighton Ave | 234.8 | PCC | Cr_Gtt | Y | 255 | N | 54.0 | 0.0 | \$31,320 | \$0 | \$31,320 | 54 | 23% |
| Good | CRB_597W | SDWK_597W | W | St Patrick St | Windsor Rd | Brighton Ave | 240.9 | PCC | Cr_Gtt | Y | 280 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 14% |
| Fair | CRB_598E | SDWK_598E | E | St Patrick St | Brighton Ave | Oak Bay Ave | 229.1 | PCC | Cr_Gtt | Y | 814 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_598W | SDWK_598W | W | St Patrick St | Brighton Ave | Oak Bay Ave | 234.3 | PCC | Cr_Gtt | Y | 268 | N | 39.0 | 0.0 | \$22,620 | \$0 | \$22,620 | 39 | 17% |
| Good | CRB_599W | SDWK_599W | W | Stonehewer Pl | West End | Falkland Rd | 307.5 | PCC | Cr_Gtt | Y | 263 | N | 42.0 | 0.0 | \$24,360 | \$0 | \$24,360 | 42 | 14% |
| Fair | CRB_601E | | E | Sunny Ln | Sunset Ave | King George Ter | 84.6 | AC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_603N | | N | Sunset Ave | Sunny Ln | Beach Dr | 104.2 | PCC | Mntbl | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Poor | CRB_603S | SDWK_603S | S | Sunset Ave | Sunny Ln | Beach Dr | 103.5 | PCC | Cr_Gtt | Y | 114 | Y | 105.0 | 0.0 | \$60,900 | \$0 | \$60,900 | 105 | 100% |
| Fair | CRB_607NS | SDWK_607NS | NS | Swanson Pl | West End | Cardiff Pl | 195.8 | PCC | Mntbl | N | 328 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 11% |
| Fair | CRB_609N | | N | Tarn Pl | Southeast End | Beach Dr | 108.5 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_609S | | S | Tarn Pl | Southeast End | Beach Dr | 99.3 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_611E | SDWK_611E | E | Thompson Ave | Nottingham Rd | Topp Ave | 89. | | | | | | | | | | | | |

**DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY**

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | | |
|----------------|-------------|--------------|------|------------------|------------------------------|--------|----------|--------|-------|-----|---------|------------------|--------|----------|-----------|-------------------|----------------|----------------|---------------|
| | | SW ID | Face | | | | | | | | | Street | Gutter | Priority | Proj? | | | PCC Length (m) | AC Length (m) |
| Fair | CRB_620E | | E | Topp Ave | Pacific Ave | 140.1 | PCC | Mntbl | N | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_620W | | W | Topp Ave | Pacific Ave | 155.0 | PCC | Mntbl | N | 0 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 4% |
| Fair | CRB_621N | SDWK_621N | N | Townley St | Foul Bay Rd | 221.2 | PCC | Mntbl | N | 509 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_621S | SDWK_621S | S | Townley St | Foul Bay Rd | 222.3 | PCC | Mntbl | N | 267 | N | | 42.0 | 0.0 | \$24,360 | \$0 | \$24,360 | 42 | 19% |
| Fair | CRB_622E | SDWK_622E | E | Transit Rd | Beach Dr | 364.6 | AC | Mntbl | Y | 37 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Poor | CRB_622W | | W | Transit Rd | Beach Dr | 360.4 | AC | Mntbl | Y | 0 | Y | | 0.0 | 360.0 | \$136,800 | \$136,800 | \$136,800 | 360 | 100% |
| Good | CRB_623E | SDWK_623E | E | Transit Rd | Central Ave | 131.3 | PCC | Mntbl | N | 535 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_623W | | W | Transit Rd | Central Ave | 125.1 | PCC | Mntbl | N | 0 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% |
| Fair | CRB_624E | SDWK_624E | E | Transit Rd | Rosario St | 88.8 | PCC | Mntbl | N | 452 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_624W | | W | Transit Rd | Rosario St | 73.6 | PCC | Mntbl | N | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_625E | SDWK_625E | E | Transit Rd | Zela St | 85.4 | PCC | Mntbl | N | 479 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_625W | | W | Transit Rd | Zela St | 78.3 | PCC | Mntbl | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_626E | SDWK_626E | E | Transit Rd | Cookman St | 84.2 | PCC | Mntbl | N | 824 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_626W | | W | Transit Rd | Cookman St | 79.5 | PCC | Mntbl | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_627E | SDWK_627E | E | Transit Rd | McNeill Ave | 40.1 | PCC | Mntbl | N | 495 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% |
| Fair | CRB_627W | SDWK_627W | W | Transit Rd | McNeill Ave | 45.8 | PCC | Mntbl | N | 494 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% |
| Fair | CRB_628E | | E | Transit Rd | Currie Rd | 132.9 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_628W | SDWK_628W | W | Transit Rd | Currie Rd | 145.3 | PCC | Mntbl | N | 518 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_630S | SDWK_630S | S | Transit Rd | St Denis St | 83.3 | PCC | Cr_Gtt | N | 382 | N | | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 14% |
| Good | CRB_635E | | E | University Woods | Foul Bay Rd | 166.6 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_635SENB | SDWK_635SENB | SENB | University Woods | Foul Bay Rd | 529.3 | PCC | Cr_Gtt | N | 762 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_635SENB | | SENB | University Woods | Foul Bay Rd | 691.0 | PCC | Cr_Gtt | N | 0 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 1% |
| Good | CRB_635WA | SDWK_635WA | W | University Woods | Foul Bay Rd | 157.8 | PCC | Mntbl | N | 759 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_637E | | E | Uplands Rd | Cadboro Bay Rd | 86.5 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_637N | SDWK_637N | N | Uplands Rd | Cadboro Bay Rd | 69.2 | PCC | Mower | Y | 7 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_638N | SDWK_638N | N | Uplands Rd | Burdick Ave | 97.1 | PCC | Mower | Y | 429 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 6% |
| Fair | CRB_638S | | S | Uplands Rd | Burdick Ave | 86.9 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_638W | SDWK_638W | W | Uplands Rd | Burdick Ave | 226.5 | PCC | Mower | Y | 30 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_639E | | E | Uplands Rd | Dewdney Ave | 229.2 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_640E_B | | E | Uplands Rd | Lansdowne Rd | 105.1 | PCC | Mower | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_640W | | W | Uplands Rd | Lansdowne Rd | 168.8 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_641E | | E | Uplands Rd | Cotswold Rd | 120.5 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_641W | | W | Uplands Rd | Cotswold Rd | 118.5 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_642E | | E | Uplands Rd | Uplands Pl | 208.1 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_642W | | W | Uplands Rd | Uplands Pl | 213.1 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_643E | | E | Uplands Rd | Weald Rd | 21.8 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_643W | | W | Uplands Rd | Weald Rd | 20.9 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_644N | | N | Uplands Rd | Cardigan Rd | 110.6 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_644S | | S | Uplands Rd | Cardigan Rd | 56.9 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_645E | SDWK_645E | E | Upper Ter | Cotswold Rd / Weald Rd | 64.1 | PCC | Mower | Y | 820 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_645N | | N | Upper Ter | Cotswold Rd / Weald Rd | 74.3 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_646E | | E | Upper Ter | Upper Ter 1 | 302.9 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_646W | | W | Upper Ter | Upper Ter 1 | 287.2 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_647E | SDWK_647E | E | Upper Ter | Uplands Rd | 378.7 | AC | Mntbl | Y | 91 | N | | 6.0 | 0.0 | \$3,480 | \$3,480 | \$3,480 | 6 | 2% |
| Fair | CRB_647W | | W | Upper Ter | Uplands Rd | 353.3 | PCC | Cr_Gtt | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_648N | SDWK_648N | N | Upper Ter 1 | Upper Ter 1 | 99.4 | PCC | Mower | Y | 710 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_648S | SDWK_648S | S | Upper Ter 1 | Upper Ter 1 | 100.8 | PCC | Mower | Y | 684 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_649N | | N | Valdez Pl | Northwest End | 112.7 | AC | Mntbl | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_650E | SDWK_650E | E | Victoria Ave | Beach Dr | 101.7 | PCC | Cr_Gtt | N | 84 | N | | 45.0 | 0.0 | \$26,100 | \$0 | \$26,100 | 45 | 44% |
| Fair | CRB_650W | | W | Victoria Ave | Beach Dr | 93.5 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_651E | SDWK_651E | E | Victoria Ave | Lafayette St | 116.4 | PCC | Cr_Gtt | N | 160 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 5% |
| Fair | CRB_651W | | W | Victoria Ave | Lafayette St | 120.3 | PCC | Cr_Gtt | N | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Good | CRB_652E | SDWK_652E | E | Victoria Ave | Guernsey St | 47.6 | PCC | Cr_Gtt | N | 164 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 6% |
| Fair | CRB_652W | | W | Victoria Ave | Guernsey St | 60.3 | PCC | Cr_Gtt | N | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 5% |
| Good | CRB_653E | SDWK_653E | E | Victoria Ave | McLaren Ave | 28.1 | PCC | Cr_Gtt | N | 690 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_653W | | W | Victoria Ave | McLaren Ave | 39.8 | PCC | Cr_Gtt | N | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 8% |
| Good | CRB_654E | SDWK_654E | E | Victoria Ave | Bartlett Ave | 75.5 | PCC | Cr_Gtt | N | 681 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_654W | | W | Victoria Ave | Bartlett Ave | 76.8 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_655E | SDWK_655E | E | Victoria Ave | Central Ave | 137.0 | PCC | Cr_Gtt | N | 358 | N | | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 11% |
| Fair | CRB_655W | | W | Victoria Ave | Central Ave | 136.0 | PCC | Cr_Gtt | N | 0 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_656E | SDWK_656E | E | Victoria Ave | Beaverbrooke St | 90.7 | PCC | Cr_Gtt | N | 384 | N | | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 13% |
| Fair | CRB_656W | | W | Victoria Ave | Beaverbrooke St | 98.5 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_657E | SDWK_657E | E | Victoria Ave | Smythe St | 51.8 | PCC | Cr_Gtt | N | 492 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 6% |
| Fair | CRB_657W | | W | Victoria Ave | Smythe St | 43.7 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_658W | | W | Victoria Ave | Pentland Rd | 13.6 | PCC | Cr_Gtt | N | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_659E | SDWK_659E | E | Victoria Ave | Pentland Rd | 91.5 | PCC | Cr_Gtt | N | 451 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 7% |
| Fair | CRB_659W | SDWK_659W | W | Victoria Ave | Pentland Rd | 97.9 | PCC | Cr_Gtt | N | 236 | N | | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_660E | SDWK_660E | E | Victoria Ave | McNeill Ave | 100.9 | PCC | Mntbl | Y | 260 | N | | 45.0 | 0.0 | \$26,100 | \$0 | \$26,100 | 45 | 45% |
| Good | CRB_660W | SDWK_660W | W | Victoria Ave | McNeill Ave | 92.9 | PCC | Cr_Gtt | Y | 16 | N | | 30.0 | 0.0 | \$17,400 | \$0 | \$17,400 | 30 | 32% |
| Good | CRB_661E | SDWK_661E | E | Victoria Ave | Sutherland Rd | 84.5 | PCC | Cr_Gtt | Y | 351 | N | | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 18% |
| Poor | CRB_661W | SDWK_661W | W | Victoria Ave | Sutherland Rd | 85.6 | PCC | Cr_Gtt | Y | 225 | Y | | 87.0 | 0.0 | \$50,460 | \$0 | \$50,460 | 87 | 100% |
| Fair | CRB_662E | SDWK_662E | E | Victoria Ave | Windsor Rd | 225.7 | PCC | Cr_Gtt | Y | 361 | N | | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 5% |
| Good | CRB_662W | SDWK_662W | W | Victoria Ave | Windsor Rd | 231.8 | PCC | Cr_Gtt | Y | 153 | N | | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_663E | SDWK_663E | E | Victoria Ave | Brighton Ave | 118.6 | PCC | Cr_Gtt | Y | 555 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_663W | SDWK_663W | W | Victoria Ave | Brighton Ave | 115.3 | PCC | Cr_Gtt | Y | 391 | N | | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 8% |
| Fair | CRB_664E | | E | Weald Rd | Lansdowne Rd / Nottingham Rd | 219.2 | AC | Mntbl | Y | 0 | N | | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |

**DISTRICT OF OAK BAY
CURB NETWORK CONDITION SUMMARY**

Sort: Street Name and Curb ID

| Curb Condition | Curb ID | Block | | From | To | Length | Material | Type | Paved | SW | Capital | Curb Replacement | | | | Recon. Length (m) | Recon. Percent | | |
|----------------|------------|-------------|------|--------------|---------------------------|--------------------------|----------|------|--------|----|---------|------------------|--------|----------------|---------------|-------------------|----------------|----------|---------|
| | | SW ID | Face | | | | | | | | | Street | Street | PCC Length (m) | AC Length (m) | | | PCC Cost | AC Cost |
| Fair | CRB_665E | | E | Weald Rd | Cotswood Rd / Upper Ter | Uplands Rd | 321.2 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_665W | | W | Weald Rd | Cotswood Rd / Upper Ter | Uplands Rd | 312.1 | PCC | Cr_Gtt | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_666W | SDWK_666W | W | Wessex Cir | Southwest End | Henderson Rd / Wessex Cr | 671.0 | PCC | Cr_Gtt | N | 720 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_666WA | | W | Wessex Cir | Southwest End | Henderson Rd / Wessex Cr | 68.6 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_667N | SDWK_667N | N | Wessex Cr | Henderson Rd / Wessex Cir | Woodburn Ave | 113.0 | PCC | Mntbl | N | 468 | N | 3.0 | 3.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Fair | CRB_667S | SDWK_667S | S | Wessex Cr | Henderson Rd / Wessex Cir | Woodburn Ave | 115.3 | PCC | Mntbl | N | 80 | Y | 81.0 | 0.0 | \$46,980 | \$0 | \$46,980 | 81 | 70% |
| Poor | CRB_668WE | SDWK_668E/W | WE | Westdowne Rd | South End | Middowne Rd | 304.3 | PCC | Mntbl | N | 42 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_669E | SDWK_669E | E | Westdowne Rd | Middowne Rd | Lansdowne Rd | 190.4 | PCC | Mntbl | N | 365 | N | 12.0 | 0.0 | \$6,960 | \$0 | \$6,960 | 12 | 6% |
| Fair | CRB_669W | SDWK_669W | W | Westdowne Rd | Middowne Rd | Lansdowne Rd | 188.0 | PCC | Mntbl | N | 693 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_670E | SDWK_670E | E | Westdowne Rd | Lansdowne Rd | Henderson Rd | 208.4 | PCC | Mntbl | Y | 517 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_670W | SDWK_670W | W | Westdowne Rd | Lansdowne Rd | Henderson Rd | 222.4 | PCC | Mntbl | Y | 331 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 8% |
| Fair | CRB_671E_A | SDWK_671E_A | E | Wilmot Pl | Oak Bay Ave | North End | 45.9 | PCC | Cr_Gtt | N | 399 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 20% |
| Fair | CRB_671E_B | SDWK_671E_B | E | Wilmot Pl | Oak Bay Ave | North End | 250.8 | PCC | Cr_Gtt | Y | 295 | N | 27.0 | 0.0 | \$15,660 | \$0 | \$15,660 | 27 | 11% |
| Good | CRB_671W_A | SDWK_671W_A | W | Wilmot Pl | Oak Bay Ave | North End | 45.2 | PCC | Cr_Gtt | N | 716 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_671W_B | SDWK_671W_B | W | Wilmot Pl | Oak Bay Ave | North End | 252.7 | PCC | Cr_Gtt | Y | 396 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 4% |
| Fair | CRB_673N | SDWK_673N | N | Windsor Rd | Victoria Ave | Roslyn Rd | 90.6 | PCC | Cr_Gtt | N | 14 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_673S | | S | Windsor Rd | Victoria Ave | Roslyn Rd | 88.3 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_674N | SDWK_674N | N | Windsor Rd | Roslyn Rd | Hampshire Rd | 79.4 | PCC | Cr_Gtt | Y | 598 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_674S | | S | Windsor Rd | Roslyn Rd | Hampshire Rd | 76.4 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_675N | SDWK_675N | N | Windsor Rd | Hampshire Rd | Monterey Ave | 120.6 | PCC | Cr_Gtt | N | 3 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_675S | | S | Windsor Rd | Hampshire Rd | Monterey Ave | 118.3 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_676N | SDWK_676N | N | Windsor Rd | Monterey Ave | Oliver St | 81.9 | PCC | Mower | N | 13 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_676S | | S | Windsor Rd | Monterey Ave | Oliver St | 80.3 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_677N | SDWK_677N | N | Windsor Rd | Oliver St | St Patrick St | 81.2 | PCC | Mower | N | 44 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_677S | | S | Windsor Rd | Oliver St | St Patrick St | 79.5 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_678N | SDWK_678N | N | Windsor Rd | St Patrick St | St David St | 80.8 | PCC | Mower | N | 53 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_678S | | S | Windsor Rd | St Patrick St | St David St | 78.7 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_679N | SDWK_679N | N | Windsor Rd | St David St | Transit Rd | 88.1 | PCC | Mower | N | 47 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_679S | | S | Windsor Rd | St David St | Transit Rd | 77.9 | PCC | Cr_Gtt | N | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Good | CRB_680N | SDWK_680N | N | Windsor Rd | Transit Rd | St Denis St | 169.2 | PCC | Mower | N | 569 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_680S | | S | Windsor Rd | Transit Rd | St Denis St | 182.7 | PCC | Cr_Gtt | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_681N_A | SDWK_681N_A | N | Windsor Rd | St Denis St | Newport Ave | 48.4 | PCC | Cr_Gtt | N | 596 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_681N_B | SDWK_681N_B | N | Windsor Rd | St Denis St | Newport Ave | 45.9 | PCC | Mower | N | 32 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 7% |
| Good | CRB_681S | SDWK_681S | S | Windsor Rd | St Denis St | Newport Ave | 89.6 | PCC | Cr_Gtt | N | 501 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 3% |
| Good | CRB_682N | SDWK_682N | N | Windsor Rd | Newport Ave | Beach Dr / Goodwin St | 59.1 | PCC | Mower | N | 411 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 15% |
| Fair | CRB_682S | SDWK_682S | S | Windsor Rd | Newport Ave | Beach Dr / Goodwin St | 76.3 | PCC | Cr_Gtt | N | 462 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 4% |
| Fair | CRB_683E | SDWK_683E | E | Woodburn Ave | South End | Wessex Cr | 240.0 | PCC | Mntbl | N | 342 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 6% |
| Fair | CRB_683W | SDWK_683W | W | Woodburn Ave | South End | Wessex Cr | 233.6 | PCC | Mntbl | N | 281 | N | 33.0 | 0.0 | \$19,140 | \$0 | \$19,140 | 33 | 14% |
| Fair | CRB_684E | SDWK_684E | E | Woodburn Ave | Wessex Cr | Dundrum Rd | 238.3 | PCC | Mntbl | N | 343 | N | 15.0 | 0.0 | \$8,700 | \$0 | \$8,700 | 15 | 6% |
| Fair | CRB_684W | SDWK_684W | W | Woodburn Ave | Wessex Cr | Dundrum Rd | 228.7 | PCC | Mntbl | N | 156 | N | 9.0 | 0.0 | \$5,220 | \$0 | \$5,220 | 9 | 4% |
| Fair | CRB_685E | SDWK_685E | E | Woodburn Ave | Dundrum Rd | North End | 230.4 | PCC | Mntbl | N | 422 | N | 6.0 | 0.0 | \$3,480 | \$0 | \$3,480 | 6 | 3% |
| Fair | CRB_685W | SDWK_685W | W | Woodburn Ave | Dundrum Rd | North End | 225.1 | PCC | Mntbl | N | 119 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 1% |
| Fair | CRB_686N | SDWK_686N | N | Woodhouse Rd | Mowat St | Dryfe St | 79.8 | PCC | Mntbl | N | 821 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_686S | SDWK_686S | S | Woodhouse Rd | Mowat St | Dryfe St | 73.8 | PCC | Mntbl | N | 687 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_687N | SDWK_687N | N | Woodhouse Rd | Dryfe St | Cadboro Bay Rd / Tod Rd | 84.8 | PCC | Mntbl | N | 822 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_687S | SDWK_687S | S | Woodhouse Rd | Dryfe St | Cadboro Bay Rd / Tod Rd | 77.9 | PCC | Mntbl | Y | 679 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_688N | | N | Woodlaw Cr | Monterey Ave | East End | 54.8 | AC | Mntbl | Y | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_689E | SDWK_689E | E | Wootton Cr | Carrick St / Henderson Rd | Kings Rd | 177.9 | PCC | Mntbl | N | 658 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_689W | SDWK_689W | W | Wootton Cr | Carrick St / Henderson Rd | Kings Rd | 144.0 | PCC | Mntbl | N | 727 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_691E | SDWK_691E | E | Yale St | Oak Bay Ave | Byron St | 190.2 | PCC | Cr_Gtt | Y | 320 | N | 21.0 | 0.0 | \$12,180 | \$0 | \$12,180 | 21 | 11% |
| Fair | CRB_691W | SDWK_691W | W | Yale St | Oak Bay Ave | Byron St | 185.0 | PCC | Cr_Gtt | Y | 475 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Poor | CRB_692WE | | WE | Yale St | Byron St | North End | 48.1 | AC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Fair | CRB_693E | SDWK_693E | E | York Pl | Oak Bay Ave | North End | 195.3 | PCC | Cr_Gtt | Y | 230 | N | 18.0 | 0.0 | \$10,440 | \$0 | \$10,440 | 18 | 9% |
| Fair | CRB_693W | | W | York Pl | Oak Bay Ave | North End | 123.9 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 2% |
| Fair | CRB_693W | | W | York Pl | Oak Bay Ave | North End | 61.9 | PCC | Cr_Gtt | Y | 0 | N | 3.0 | 0.0 | \$1,740 | \$0 | \$1,740 | 3 | 5% |
| Good | CRB_694N | | N | Zela St | St Patrick St | Transit Rd | 144.4 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |
| Good | CRB_694S | | S | Zela St | St Patrick St | Transit Rd | 143.3 | PCC | Mntbl | N | 0 | N | 0.0 | 0.0 | \$0 | \$0 | \$0 | 0 | 0% |

APPENDIX D

Missing and Failed Ramps Listing

DISTRICT OF OAK BAY
MISSING AND FAILED RAMPS LIST

Sort: Street Name and Sidewalk ID

| Block | | | | | | Sidewalk | Missing | Failed |
|-------------|----------|------|----------------|----------------------------|---------------------------------|----------|---------|--------|
| Sidewalk ID | Curb ID | Face | Street | From | To | Material | Ramps | Ramps2 |
| SDWK_7E | CRB_7E | E | Ashdowne Rd | Sandowne Rd | Middowne Rd | AC | 1 | 0 |
| SDWK_7W | CRB_7W | W | Ashdowne Rd | Sandowne Rd | Middowne Rd | AC | 3 | 0 |
| SDWK_8N | CRB_8N | N | Avondale Rd | Plymouth Rd | Cardiff Pl | PCC | 1 | 0 |
| SDWK_9S | CRB_9S | S | Avondale Rd | Cardiff Pl | Henderson Rd | PCC | 1 | 0 |
| SDWK_13E | CRB_13E | E | Beach Dr | Denison Rd / Fairfield Rd | Mountjoy Ave | PCC | 1 | 0 |
| SDWK_29E | CRB_29E | E | Beach Dr | Satellite St | Orchard Ave | PCC | 0 | 1 |
| SDWK_36E | CRB_36E | E | Beach Dr | Shady Ln | Somass Dr | PCC | 0 | 1 |
| SDWK_49E | CRB_49E | E | Beach Dr | Valdez Pl | Tarn Pl | PCC | 1 | 0 |
| SDWK_52E | CRB_52E | E | Beach Dr | Lansdowne Rd | Humber Rd | PCC | 2 | 0 |
| SDWK_65S | CRB_65S | S | Bowker Ave | St Ann St | Lulie St | PCC | 0 | 2 |
| SDWK_76S | CRB_76S | S | Brighton Ave | Hewlett Pl | Victoria Ave | PCC | 1 | 0 |
| SDWK_107E | CRB_107E | E | Cadboro Bay Rd | Estevan Ave | Pacific Ave | PCC | 0 | 2 |
| SDWK_119EB | CRB_119E | E | Cadboro Bay Rd | Chiltern Pl | Cedar Hill Cross Rd / Upper Ter | PCC | 1 | 0 |
| SDWK_95N_A | CRB_95N | N | Cadboro Bay Rd | Foul Bay Rd | Bee St / Florence St | PCC | 0 | 1 |
| SDWK_124E | CRB_124E | E | Cardiff Pl | Kendal Ave | Renfrew Rd | PCC | 1 | 0 |
| SDWK_124W | CRB_124W | W | Cardiff Pl | Kendal Ave | Renfrew Rd | PCC | 1 | 0 |
| SDWK_127W | CRB_127W | W | Cardiff Pl | Redwood Ave | Swanson Pl | PCC | 1 | 0 |
| SDWK_128W | CRB_128W | W | Cardiff Pl | Swanson Pl | Frederick Norris Rd | PCC | 3 | 0 |
| SDWK_133S | CRB_133S | S | Cavendish Ave | Hampshire Rd / Musgrave St | Beach Dr | PCC | 1 | 0 |
| SDWK_138N | CRB_138N | N | Central Ave | Mountjoy Ave | Falkland Rd | AC | 2 | 0 |
| SDWK_138S | CRB_138S | S | Central Ave | Mountjoy Ave | Falkland Rd | AC | 2 | 0 |
| SDWK_139N | CRB_139N | N | Central Ave | Falkland Rd | Victoria Ave | AC | 1 | 0 |
| SDWK_139S | CRB_139S | S | Central Ave | Falkland Rd | Victoria Ave | AC | 1 | 0 |
| SDWK_141S | CRB_141S | S | Central Ave | Hampshire Rd | Monterey Ave | AC | 1 | 0 |
| SDWK_142S | CRB_142S | S | Central Ave | Monterey Ave | Oliver St | AC | 1 | 0 |
| SDWK_145N | CRB_145N | N | Central Ave | Lyn Cr | Transit Rd | AC | 1 | 0 |
| SDWK_146N | CRB_146N | N | Central Ave | Transit Rd | Island Rd | AC | 1 | 0 |
| SDWK_147N | CRB_147N | N | Central Ave | Island Rd | Byng St | AC | 2 | 0 |
| SDWK_147S | CRB_147S | S | Central Ave | Island Rd | Byng St | AC | 1 | 0 |
| SDWK_148N | CRB_148N | N | Central Ave | Byng St | Linkleas Ave | AC | 2 | 0 |
| SDWK_148S | CRB_148S | S | Central Ave | Byng St | Linkleas Ave | AC | 1 | 0 |
| SDWK_149N | CRB_149N | N | Central Ave | Linkleas Ave | Newport Ave | AC | 2 | 0 |
| SDWK_149S | CRB_149S | S | Central Ave | Linkleas Ave | Newport Ave | AC | 1 | 0 |
| SDWK_157S | CRB_157S | S | Cotswold Rd | Uplands Rd | Upper Ter / Weald Rd | PCC | 1 | 0 |
| SDWK_163S | CRB_163S | S | Cranmore Rd | St Ann St | Lulie St | PCC | 1 | 0 |
| SDWK_164S | CRB_164S | S | Cranmore Rd | Lulie St | Monteith St | PCC | 1 | 0 |

DISTRICT OF OAK BAY
MISSING AND FAILED RAMPS LIST

Sort: Street Name and Sidewalk ID

| Block | | | | | | Sidewalk | Missing | Failed |
|-------------|------------|------|---------------------|------------------------|-----------------------|----------|---------|--------|
| Sidewalk ID | Curb ID | Face | Street | From | To | Material | Ramps | Ramps2 |
| SDWK_179N | CRB_179N | N | Dalhousie St | Eastdowne Rd | Mowat St | PCC | 1 | 0 |
| SDWK_179S | CRB_179S | S | Dalhousie St | Eastdowne Rd | Mowat St | PCC | 1 | 0 |
| SDWK_190W | CRB_190W | W | Devon Rd | South End | Dover Rd | PCC | 1 | 0 |
| SDWK_191W | CRB_191W | W | Devon Rd | Dover Rd | Nottingham Rd | AC | 1 | 0 |
| SDWK_201E | CRB_201E | E | Dover Rd | Nottingham Rd | Devon Rd | PCC | 1 | 0 |
| SDWK_202W | CRB_202W | W | Dryfe St | Dalhousie St | Woodhouse Rd | PCC | 0 | 1 |
| SDWK_203E | CRB_203E | E | Dufferin Ave | Cadboro Bay Rd | Nottingham Rd | AC | 1 | 0 |
| SDWK_203W | CRB_203W | W | Dufferin Ave | Cadboro Bay Rd | Nottingham Rd | PCC | 1 | 0 |
| SDWK_204W | CRB_204W | W | Dufferin Ave | Nottingham Rd | Mayhew St | PCC | 1 | 0 |
| SDWK_207E | CRB_207E | E | Dunlevy St | Dalhousie St | Estevan Ave | AC | 2 | 0 |
| SDWK_208E | CRB_208E | E | Dunlevy St | Estevan Ave | Burdick Ave | PCC | 1 | 0 |
| SDWK_208W | CRB_208W | W | Dunlevy St | Estevan Ave | Burdick Ave | PCC | 1 | 0 |
| SDWK_209E | CRB_209E | E | Dunlevy St | Burdick Ave | Dorset Rd | PCC | 1 | 0 |
| SDWK_209W | CRB_209W | W | Dunlevy St | Burdick Ave | Dorset Rd | PCC | 1 | 0 |
| SDWK_210W | CRB_210W | W | Eastdowne Rd | Cadboro Bay Rd | Bowker Ave | PCC | 1 | 0 |
| SDWK_212W | CRB_212W | W | Eastdowne Rd | Cranleigh Pl | Dalhousie St | PCC | 1 | 0 |
| SDWK_222E | CRB_222E | E | Eastdowne Rd | Neil St | Sandowne Rd | AC | 1 | 0 |
| SDWK_224W | CRB_224W | W | Eastdowne Rd | Middowne Rd | Oakdowne Rd | AC | 1 | 0 |
| SDWK_240N | CRB_240N | N | Estevan Ave | Dunlevy St | Heron St | PCC | 1 | 0 |
| SDWK_241N | CRB_241N | N | Estevan Ave | Heron St | Lincoln Rd | PCC | 2 | 0 |
| SDWK_242N | CRB_242N | N | Estevan Ave | Lincoln Rd | Beach Dr | PCC | 1 | 0 |
| SDWK_244E | CRB_244E | E | Exeter Rd | Lansdowne Rd | Beach Dr | PCC | 2 | 0 |
| SDWK_246W | CRB_246W | W | Fairfield Rd | West End | Beach Dr / Denison Rd | AC | 1 | 0 |
| SDWK_287W | CRB_287W | W | Foul Bay Rd | Gibbs Rd | Henderson Rd | PCC | 1 | 0 |
| SDWK_290S | CRB_290S | S | Frederick Norris Rd | Kelsey Pl | Cardiff Pl | PCC | 1 | 0 |
| SDWK_293W | CRB_293W | W | Gibbs Rd | Foul Bay Rd | Northeast End | PCC | 1 | 0 |
| SDWK_295S | CRB_295S | S | Goldsmith St | Bee St | East End | PCC | 1 | 0 |
| SDWK_299N | CRB_299N | N | Granite St | Foul Bay Rd | Mitchell St | PCC | 1 | 0 |
| SDWK_301N_A | CRB_301N_A | N | Granite St | Victoria Ave | Hampshire Rd | PCC | 0 | 1 |
| SDWK_332E | CRB_332E | E | Henderson Rd | Newton St | Carnarvon St | PCC | 1 | 0 |
| SDWK_335E | CRB_335E | E | Henderson Rd | Allenby St | Neil St | PCC | 1 | 0 |
| SDWK_335W | CRB_335W | W | Henderson Rd | Allenby St | Neil St | PCC | 1 | 0 |
| SDWK_336E | CRB_336E | E | Henderson Rd | Neil St | Middowne Rd | AC | 1 | 0 |
| SDWK_340E | CRB_340E | E | Henderson Rd | Wessex Cir / Wessex Cr | Foul Bay Rd | PCC | 1 | 0 |
| SDWK_340W | CRB_340W | W | Henderson Rd | Wessex Cir / Wessex Cr | Foul Bay Rd | PCC | 2 | 0 |
| SDWK_341E | CRB_341E | E | Henderson Rd | Foul Bay Rd | Dundrum Rd | PCC | 1 | 0 |

DISTRICT OF OAK BAY
MISSING AND FAILED RAMPS LIST

Sort: Street Name and Sidewalk ID

| Block | | | | | | Sidewalk | Missing | Failed |
|-------------|-----------|------|-----------------------|--------------------------|---------------------|----------|---------|--------|
| Sidewalk ID | Curb ID | Face | Street | From | To | Material | Ramps | Ramps2 |
| SDWK_342E | CRB_342E | E | Henderson Rd | Dundrum Rd | Kendal Ave | PCC | 1 | 0 |
| SDWK_344E | CRB_344E | E | Henderson Rd | Avondale Rd | Arden Rd | PCC | 0 | 1 |
| SDWK_344W | CRB_344W | W | Henderson Rd | Avondale Rd | Arden Rd | PCC | 1 | 0 |
| SDWK_345E | CRB_345E | E | Henderson Rd | Arden Rd | Murdoch Cr | PCC | 1 | 0 |
| SDWK_346E | CRB_346E | E | Henderson Rd | Murdoch Cr | Murdoch Cr | PCC | 2 | 0 |
| SDWK_347E | CRB_347E | E | Henderson Rd | Murdoch Cr | Frederick Norris Rd | PCC | 1 | 0 |
| SDWK_363WE | CRB_363WE | WE | Kelsey Pl | South End | Frederick Norris Rd | PCC | 1 | 0 |
| SDWK_365S | CRB_365S | S | Kendal Ave | Plymouth Rd | Cardiff Pl | PCC | 1 | 0 |
| SDWK_366S | CRB_366S | S | Kendal Ave | Cardiff Pl | Cardiff Pl | PCC | 1 | 0 |
| SDWK_367S | CRB_367S | S | Kendal Ave | Cardiff Pl | Henderson Rd | PCC | 1 | 0 |
| SDWK_379N | CRB_379N | N | Lansdowne Rd | Westdowne Rd | Henderson Rd | PCC | 1 | 0 |
| SDWK_383N | CRB_383N | N | Lansdowne Rd | Cadboro Bay Rd | Uplands Rd | PCC | 1 | 0 |
| SDWK_385N | CRB_385N | N | Lansdowne Rd | Nottingham Rd / Weald Rd | Midland Rd | PCC | 1 | 0 |
| SDWK_385S | CRB_385S | S | Lansdowne Rd | Nottingham Rd / Weald Rd | Midland Rd | PCC | 1 | 0 |
| SDWK_387S | CRB_387S | S | Lansdowne Rd | Midland Rd | Ripon Rd | PCC | 3 | 0 |
| SDWK_388N | CRB_388N | N | Lansdowne Rd | Ripon Rd | Southdowne Rd | PCC | 1 | 0 |
| SDWK_389N | CRB_389N | N | Lansdowne Rd | Southdowne Rd | Norfolk Rd | PCC | 1 | 0 |
| SDWK_391N | CRB_391N | N | Lansdowne Rd | Rutland Rd | Exeter Rd | PCC | 1 | 0 |
| SDWK_392N | CRB_392N | N | Lansdowne Rd | Exeter Rd | Beach Dr | PCC | 1 | 0 |
| SDWK_394N | CRB_394N | N | Larkdowne Rd | Sandowne Rd | Middowne Rd | PCC | 2 | 0 |
| SDWK_395W | CRB_395W | W | Larkdowne Rd | Middowne Rd | Lansdowne Rd | PCC | 1 | 0 |
| SDWK_420S | CRB_420S | S | McNeill Ave | Foul Bay Rd | Runnymede Pl | PCC | 1 | 0 |
| SDWK_421S | CRB_421S | S | McNeill Ave | Runnymede Pl | Falkland Rd | PCC | 1 | 0 |
| SDWK_428N | CRB_428N | N | McNeill Ave | Monterey Ave | Oliver St | PCC | 0 | 1 |
| SDWK_431N | CRB_431N | N | McNeill Ave | St David St | Transit Rd | PCC | 0 | 1 |
| SDWK_431S | CRB_431S | S | McNeill Ave | St David St | Transit Rd | PCC | 0 | 1 |
| SDWK_441N | CRB_441N | N | Middowne Rd | Larkdowne Rd | Oakdowne Rd | AC | 1 | 0 |
| SDWK_441S | CRB_441S | S | Middowne Rd | Larkdowne Rd | Oakdowne Rd | AC | 1 | 0 |
| SDWK_442N | CRB_442N | N | Middowne Rd | Oakdowne Rd | Ashdowne Rd | AC | 1 | 0 |
| SDWK_443N | CRB_443N | N | Middowne Rd | Ashdowne Rd | Eastdowne Rd | AC | 2 | 0 |
| SDWK_444E | CRB_444E | E | Midland Rd | Dorset Rd | Northeast End | PCC | 1 | 0 |
| SDWK_449S | CRB_449S | S | Midland Rd Roundabout | Midland Rd | Ripon Rd 1 | PCC | 2 | 0 |
| SDWK_450N | CRB_450N | N | Midland Rd Roundabout | Ripon Rd 1 | Midland Rd | PCC | 1 | 0 |
| SDWK_452S | CRB_452S | S | Midland Rd Roundabout | Upper Ter 1 | Southwest End | PCC | 1 | 1 |
| SDWK_479E | CRB_479E | E | Musgrave St | Burdick Ave | Dewdney Ave | AC | 1 | 0 |
| SDWK_479W | CRB_479W | W | Musgrave St | Burdick Ave | Dewdney Ave | AC | 2 | 0 |

DISTRICT OF OAK BAY
MISSING AND FAILED RAMPS LIST

Sort: Street Name and Sidewalk ID

| Block | | | | | | Sidewalk | Missing | Failed |
|-------------|-----------|------|---------------|-----------------------|--|----------|---------|--------|
| Sidewalk ID | Curb ID | Face | Street | From | To | Material | Ramps | Ramps2 |
| SDWK_484W | CRB_484W | W | Newport Ave | Beach Dr | Island Rd | PCC | 2 | 0 |
| SDWK_485E | CRB_485E | E | Newport Ave | Island Rd | Central Ave | PCC | 0 | 1 |
| SDWK_485W | CRB_485W | W | Newport Ave | Island Rd | Central Ave | PCC | 1 | 0 |
| SDWK_489E | CRB_489E | E | Newport Ave | Margate Ave | Orchard Ave | PCC | 0 | 1 |
| SDWK_489W | CRB_489W | W | Newport Ave | Margate Ave | Orchard Ave | PCC | 0 | 1 |
| SDWK_491W | CRB_491W | W | Newport Ave | Currie Rd | Windsor Rd | PCC | 0 | 1 |
| SDWK_492W | CRB_492W | W | Newport Ave | Windsor Rd | Transit Rd | PCC | 0 | 1 |
| SDWK_494W | CRB_494W | W | Newport Ave | Beresford St | Oak Bay Ave / St David St | PCC | 0 | 1 |
| SDWK_495N | CRB_495N | N | Newton St | Foul Bay Rd | Henderson Rd | AC | 1 | 0 |
| SDWK_495S | CRB_495S | S | Newton St | Foul Bay Rd | Henderson Rd | PCC | 1 | 0 |
| SDWK_496S | CRB_496S | S | Newton St | Henderson Rd | Eastdowne Rd | AC | 2 | 0 |
| SDWK_498W | CRB_498W | W | Norfolk Rd | Lansdowne Rd | Ripon Rd | PCC | 1 | 0 |
| SDWK_501S | CRB_501S | S | Nottingham Rd | Burdick Ave | Dewdney Ave | PCC | 1 | 0 |
| SDWK_503S | CRB_503S | S | Nottingham Rd | Dorset Rd | Dover Rd | PCC | 1 | 0 |
| SDWK_505S | CRB_505S | S | Nottingham Rd | Devon Rd | Lansdowne Rd / Weald Rd | PCC | 1 | 0 |
| SDWK_515N | CRB_515N | N | Oak Bay Ave | Oliver St | St Patrick St | PCC | 1 | 0 |
| SDWK_520E | CRB_520E | E | Oakdowne Rd | Sandowne Rd | Middowne Rd | AC | 2 | 0 |
| SDWK_520W | CRB_520W | W | Oakdowne Rd | Sandowne Rd | Middowne Rd | AC | 1 | 0 |
| SDWK_541E | CRB_541E | E | Plymouth Rd | Kendal Ave | Renfrew Rd | PCC | 2 | 0 |
| SDWK_541W | CRB_541W | W | Plymouth Rd | Kendal Ave | Renfrew Rd | PCC | 1 | 0 |
| SDWK_542E | CRB_542E | E | Plymouth Rd | Renfrew Rd | Avondale Rd | PCC | 2 | 0 |
| SDWK_543E | CRB_543E | E | Plymouth Rd | Avondale Rd | Redwood Ave | PCC | 1 | 0 |
| SDWK_543WA | CRB_543WA | W | Plymouth Rd | Avondale Rd | Redwood Ave | PCC | 1 | 0 |
| SDWK_543WB | CRB_543WB | W | Plymouth Rd | Avondale Rd | Redwood Ave | PCC | 1 | 0 |
| SDWK_550S | CRB_550S | S | Rattenbury Pl | West End | Redwood Ave | PCC | 1 | 0 |
| SDWK_552W | CRB_552W | W | Redwood Ave | Plymouth Rd | Rattenbury Pl | PCC | 1 | 0 |
| SDWK_553W | CRB_553W | W | Redwood Ave | Rattenbury Pl | Pelly Pl | PCC | 1 | 0 |
| SDWK_554E | CRB_554E | E | Redwood Ave | Pelly Pl | Frederick Norris Pl / Frederick Norris R | PCC | 1 | 0 |
| SDWK_554W | CRB_554W | W | Redwood Ave | Pelly Pl | Frederick Norris Pl / Frederick Norris R | PCC | 1 | 0 |
| SDWK_555N | CRB_555N | N | Renfrew Rd | Plymouth Rd | Cardiff Pl | PCC | 1 | 0 |
| SDWK_555S | CRB_555S | S | Renfrew Rd | Plymouth Rd | Cardiff Pl | PCC | 1 | 0 |
| SDWK_563N | CRB_563N | N | Ripon Rd 1 | Midland Rd Roundabout | Ripon Rd | PCC | 1 | 0 |
| SDWK_563S | CRB_563S | S | Ripon Rd 1 | Midland Rd Roundabout | Ripon Rd | PCC | 1 | 0 |
| SDWK_564N | CRB_564N | N | Rosario St | St Patrick St | Transit Rd | PCC | 0 | 1 |
| SDWK_570E | CRB_570E | E | Rutland Rd | Lansdowne Rd | Beach Dr | PCC | 1 | 1 |
| SDWK_603S | CRB_603S | S | Sunset Ave | Sunny Ln | Beach Dr | PCC | 1 | 0 |

DISTRICT OF OAK BAY
MISSING AND FAILED RAMPS LIST

Sort: Street Name and Sidewalk ID

| Block | | | | | | Sidewalk | Missing | Failed |
|-------------|----------|------|--------------|---------------------------|--------------------------------------|----------|---------|--------|
| Sidewalk ID | Curb ID | Face | Street | From | To | Material | Ramps | Ramps2 |
| SDWK_640E | CRB_640E | E | Uplands Rd | Lansdowne Rd | Cotswold Rd | PCC | 1 | 0 |
| SDWK_647E | CRB_647E | E | Upper Ter | Uplands Rd | Cadboro Bay Rd / Cedar Hill Cross Rd | PCC | 2 | 0 |
| SDWK_650E | CRB_650E | E | Victoria Ave | Beach Dr | Lafayette St | PCC | 2 | 0 |
| SDWK_651E | CRB_651E | E | Victoria Ave | Lafayette St | Guernsey St | PCC | 1 | 0 |
| SDWK_652E | CRB_652E | E | Victoria Ave | Guernsey St | McLaren Ave | PCC | 1 | 0 |
| SDWK_662W | CRB_662W | W | Victoria Ave | Windsor Rd | Brighton Ave | PCC | 1 | 0 |
| SDWK_667S | CRB_667S | S | Wessex Cr | Henderson Rd / Wessex Cir | Woodburn Ave | PCC | 2 | 0 |
| SDWK_684W | CRB_684W | W | Woodburn Ave | Wessex Cr | Dundrum Rd | PCC | 1 | 0 |
| SDWK_685W | CRB_685W | W | Woodburn Ave | Dundrum Rd | North End | PCC | 1 | 0 |

APPENDIX E

5-Year Capital Replacement Plan

5-Year Capital Replacement Plan

Project: District of Oak Bay -2023 Sidewalk and Curb Condition Assessment

Date: May 1, 2024

| Project # | Condition | Sidewalk ID | Curb ID | Road Side | Street | From | To | Length (m) | Material | Curb Type | # Letdowns Required | Replacement Length (m) | Replacement % | Sidewalk Replacement Cost | Curb Replacement Cost | Total Replacement Cost |
|-----------|-----------|-------------|-----------|-----------|----------------|-----------------------|---------------|------------|----------|--------------|---------------------|------------------------|---------------|---------------------------------------|-----------------------|------------------------|
| 1 | Very Poor | SDWK_491W | CRB_491W | W | Newport Ave | Currie Rd | Windsor Rd | 149 | Concrete | Barrier Curb | 1 | 149 | 100% | \$ 128,500 | \$ 87,000 | \$ 215,500 |
| 2 | Fair | SDWK_509S_A | CRB_509S | S | Oak Bay Ave | Yale St | Wilmot Pl | 34 | Concrete | Barrier Curb | 0 | 18 | 53% | \$ 14,580 | \$ 10,400 | \$ 24,980 |
| 3 | Failed | SDWK_328N | CRB_328N | N | Haultain St | Henderson Rd | Eastdowne Rd | 159 | Asphalt | Mountable | 0 | 114 | 72% | \$ 98,790 | \$ 91,640 | \$ 190,430 |
| 4 | Poor | | CRB_398EA | E | Licolln St | Burdick Ave | Lane ROW | 24 | Concrete | Mountable | 0 | 24 | 100% | \$ - | \$ 13,920 | \$ 13,920 |
| | | | | | | | | | | | | | | Year 1 Construction | | \$ 444,830 |
| | | | | | | | | | | | | | | Year 1 Design | | \$ 55,170 |
| 5 | Poor | | CRB_529N | N | Orchard Ave | Newport Ave | Deal St | 90 | Concrete | Mountable | 0 | 90 | 100% | \$ - | \$ 52,200 | \$ 52,200.00 |
| 6 | Serious | SDWK_484E | CRB_484E | E | Newport Ave | Beach Dr | Island Rd | 277 | Concrete | Barrier Curb | 0 | 277 | 100% | \$ 244,181 | \$ 161,820 | \$ 406,001 |
| | | | | | | | | | | | | | | Year 2 Construction | | \$ 458,201 |
| | | | | | | | | | | | | | | Year 2 Design | | \$ 41,799 |
| 7 | Poor | | CRB_460W | W | Monterey Ave | Central Ave | Tinto St | 311 | Concrete | Mountable | 0 | 311 | 100% | \$ - | \$ 180,960 | \$ 180,960 |
| 8 | Poor | | CRB_622W | W | Transit Rd | Beach Dr | Central Ave | 360 | Concrete | Mountable | 0 | 360 | 100% | \$ - | \$ 136,800 | \$ 136,800 |
| 9 | Very Poor | SDWK_96N | CRB_96N | N | Cadboro Bay Rd | Bee St / Florence St | Epworth St | 83 | Concrete | Barrier Curb | 0 | 83 | 100% | \$ 68,402 | \$ 53,940 | \$ 122,342 |
| | | | | | | | | | | | | | | Year 3 Construction | | \$ 440,102 |
| | | | | | | | | | | | | | | Year 3 Design | | \$ 59,898 |
| 10 | Very Poor | SDWK_104E | CRB_104E | E | Cadboro Bay Rd | Tod Rd / Woodhouse Rd | Hamiota St | 41 | Concrete | Barrier Curb | 0 | 41 | 100% | \$ 34,020 | \$ 24,360 | \$ 58,380 |
| 11 | Poor | SDWK_383N | CRB_383N | N | Lansdowne Rd | Cadboro Bay Rd | Uplands Dr | 189 | Concrete | Mountable | 0 | 189 | 100% | \$ - | \$ 109,620 | \$ 109,620 |
| 12 | Poor | SDWK_21S | CRB_21S | S | Beach Dr | Monterey Ave | Oliver St | 147 | Concrete | Barrier Curb | 0 | 147 | 100% | \$ 119,287.00 | \$ 85,260 | \$ 204,547 |
| 13 | Poor | | CRB_469W | W | Mountjoy Ave | Central Ave | Runnymede Ave | 205 | Asphalt | Mountable | 0 | 205 | 100% | \$ - | \$ 77,900 | \$ 77,900 |
| | | | | | | | | | | | | | | Year 4 Construction | | \$ 450,447 |
| | | | | | | | | | | | | | | Year 4 Design | | \$ 49,553 |
| 14 | Poor | | CRB_529N | S | Orchard Ave | Newport Ave | Deal St | 92 | Concrete | Mountable | 0 | 92 | 100% | \$ - | \$ 53,940 | \$ 53,940.00 |
| 15 | Poor | SDWK_484W | CRB_484W | W | Newport Ave | Beach Dr | Island Rd | 280 | Concrete | Barrier Curb | 0 | 280 | 100 | \$ 240,559 | \$ 161,820 | \$ 402,379 |
| | | | | | | | | | | | | | | Year 5 Construction | | \$ 456,319 |
| | | | | | | | | | | | | | | Year 5 Design | | \$ 43,681 |
| | | | | | | | | | | | | | | Total Construction Cost | | \$ 2,249,899 |
| | | | | | | | | | | | | | | Total Design Cost | | \$ 250,101 |
| | | | | | | | | | | | | | | Total Capital Replacement Cost | | \$ 2,500,000 |

APPENDIX F

Sidewalk and Curb Inspection Manual



a division of Englobe

Inspection Manual for:

DISTRICT OF OAK BAY

SIDEWALK AND CURB CONDITION ASSESSMENT

Date: November 20, 2023

Project #: 9404-001-00

Proud of Our Past... Building the Future

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1.0 CONCRETE SIDEWALK CONDITIONS

1.1 FAULTING (HEAVING)

Faulting is the difference in elevation across a joint or crack. Some of the common causes of faulting include:

- Settlement due to weak subgrades,
- Pumping or loss of material from underneath the slab,
- Tree roots lifting slabs.

Severity levels are defined by the difference in elevation across the joint or crack.

Faulting, Low Severity

Low severity faulting is present when the difference in elevation of a joint or crack that is 3 mm to 10 mm.



Faulting, Medium Severity

Medium severity faulting is present when the difference in elevation of a joint or crack is between 10 mm and 19 mm.



Faulting, High Severity

High severity faulting is present when the difference in elevation of a joint or crack is 19 mm or above along Oak Bay Avenue and 25mm or above for all other sidewalks.



1.2 LINEAR CRACKING

Linear cracking is when a slab is divided into two (2) or three (3) pieces. This is typically caused by a combination of:

- Deficient jointing (pattern or construction),
- Thermal gradient curling,
- Repeated freeze thaw cycles.

Linear Crack, Low Severity

Low severity linear cracking is defined as non-filled cracks less than 10 mm in width, without crack-sealing.



Linear Crack, Medium Severity

A medium severity linear crack is present when one of the following conditions exist:

- Non-filled crack with a width between 10 mm and 20 mm,
- Non-filled crack less than 20 mm in width and faulting less than 19 mm,
- Filled crack with faulting less than 19 mm.



Linear Crack, High Severity

A high severity linear crack is present when one of the following conditions exist:

- Non-filled crack with a width greater than 21 mm,
- Filled or non-filled crack of any width with faulting greater than 20 mm.



1.3 SHATTER CRACK

A shatter crack is present when slabs are divided by cracks into four (4) or more pieces due to overloading and/or inadequate support.

Shatter, Low Severity

A low severity shatter crack is present when a slab is cracked into four (4) to five (5) pieces.



Shatter, Medium Severity

A medium severity shatter crack is present when a slab is cracked into six (6) to eight (8) pieces.



Shatter, High Severity

A high severity shatter crack is present when a slab is cracked into more than eight (8) pieces.



1.4 SCALING

Scaling is defined as a loss of surface material and is typically caused by a freeze-thaw cycle on insufficiently air entrained mixtures, and salt attack caused by deicing salts. Scaling can also be caused by poor construction practices, including inadequate curing or over-finishing of the surface. Scaling is characterized by flaking or peeling of the finished concrete surface.

Scaling, Low Severity

Low severity scaling occurs when there is a noticeable loss of material, with a minor loss of surface fines. Vertical faces of the aggregate cannot be observed.



Scaling, Medium Severity

Medium severity scaling is occurring when there is a shallow disintegration with no loss of concrete thickness. The vertical faces of the coarse aggregate are exposed up to half of the aggregate size with limited pop-outs of the individual aggregate.



Scaling, High Severity

High severity scaling is occurring when there is a noticeable deterioration of the concrete thickness due to loss of fines with a rough surface.



1.5 CORNER BREAK

A corner break is defined as a crack that has an approximate 45-degree angle, and is generally near the corner of the slab.

Corner Break, Low Severity

A low severity crack is not spalled for more than 10 percent of the length of the crack, there is no measurable faulting, and the corner piece is not broken into two or more pieces and has no loss of material and no patching.



Corner Break, Medium Severity

A medium severity corner Break is occurring when a crack is spalled at a low severity for more than 10 percent of its total length, or the faulting of the crack or joint is <13 mm, and the corner piece is not broken into two or more pieces.



Corner Break, High Severity

A high severity corner break is occurring when a crack is spalled at moderate to high severity for more than 10 percent of its total length, or faulting of the crack or joint is ≥ 13 mm, or the corner piece is broken into two or more pieces, or contains patch material.



1.6 SPALLING – JOINT

Joint spalling is defined as the breakdown (chipping or fraying) of the slab edges within 0.6 m.

Joint Spalling, Low Severity

Low severity spalling is occurring when pieces are tight and cannot be easily removed.



Joint Spalling, Medium Severity

Medium severity joint spalling is occurring when spalled pieces are loose and some can be removed. Removed pieces are less than 100 mm in width.



Joint Spalling, High Severity

High severity spalling is occurring when most of the pieces are missing and have a width of 100 mm or greater.



1.7 SPALLING – CORNER

Corner spalling is defined as the breakdown (chipping or fraying) of the slab within 0.6 m of the corner.

Corner Spalling, Low Severity

Low severity spalling is occurring when pieces are tight and cannot be easily removed. Width and length of the affected area is less than 100 mm.



Corner Spalling, Medium Severity

Medium severity spalling is occurring when pieces are loose, and some can be removed. Removed pieces are less than 100 mm in width.



Corner Spalling, High Severity

High severity spalling is occurring when most of the spalled pieces are missing and have a width of 100 mm or greater.



1.8 PATCHING – LARGE (OVER 3 M)

A patch is an area where the original material has been removed and replaced by a filler material or additional material applied to the surface after original construction.

Large Patching, Low Severity

Low severity large patching is present when a patch has a low severity distress of any type, and no measurable faulting or settlement.



Large Patching, Medium Severity

Medium severity large patching is present when a patch has a moderate severity distress of any type; or faulting or settlement up to 6 mm.



Large Patching, High Severity

High severity large patching is present when a patch has a high severity distress of any type, or faulting or settlement ≥ 6 mm.



1.9 PATCHING – SMALL (LESS THAN 3 M)

A small patch is an area where the original material has been removed and replaced by a filler material or additional material applied to the surface after original construction.

Small Patching, Low Severity

Low severity small patching is present when the area has a low severity distress of any type and no measurable faulting or settlement.



Small Patching, Medium Severity

Medium severity small patching is present when a patch is moderately deteriorated. Patch material can be dislodged with considerable effort.



SMALL PATCHING, HIGH SEVERITY

High severity small patching is present when a patch is badly deteriorated. Original distress is evident.



1.10 UTILITY COVERS

Utility covers are to be assessed for interaction with the sidewalk. If there is a hazard it is noted in the sidewalk segment.

2.0 CURB AND GUTTER

Table 2.1 provides a guideline for describing severity and extent of curb and gutter deterioration.

Table 2.1: PCC Curb and Gutter Evaluation

| CONDITION | DESCRIPTION |
|------------|--|
| Acceptable | New or repaired curb, slightly spalled, cracked or distorted, catch basins are level with pan. |
| Fair | Cracking, spalling, settling around catch basins, needs minor repair, catch basins have minor elevation difference from pan. |
| Poor | Badly cracked, spalled, settled, or disintegrated pavement level almost at top of curb, catch basin has major elevation difference from pan. |
| No curbs | No curbs are present |

3.0 ACCESSIBILITY

The accessibility rating is used to evaluate letdowns, sidewalk widths, and walkway incursion. The incursion and sidewalk widths for a sidewalk are as follows:

TABLE C-5 // PEDESTRIAN THROUGH ZONE RECOMMENDED WIDTHS

| Land Use Context | Road Type | Separation | Desirable (m) | Constrained Limit (m)* |
|---|----------------------|----------------------------|---------------|------------------------|
| Single- Family Residential | Local | Non-Separated or Separated | 1.8 | 1.8 |
| | Collector/Arterial** | Separated | 1.8 | 1.8 |
| Multi- Family Residential | Local | Non-Separated or Separated | 2.1 | 1.8 |
| | Collector/Arterial** | Separated | 2.4 | 1.8 |
| Industrial | Any** | Separated | 2.1 | 1.8 |
| Commercial | Any** | Separated | 2.4-3.0 | 2.1 |
| Area of high pedestrian activity (including temporary, special event, or seasonal)*** | Any | Separated | 3.0-4.0 | 2.4 |

* The absolute minimum width of the Pedestrian Through Zone is 1.5 metres, which should only be used under constrained conditions for distances under 100 metres

** Non-separated sidewalks are not recommended on collector, arterial, or industrial roads with motor vehicle speeds greater than 30 km/h (see **Chapter C.1**). If non-separated sidewalks cannot be avoided due to site constraints, a minimum of 0.5 metres may be added to the Pedestrian Through Zone width to provide extra separation from motor vehicles.

*** Areas of high pedestrian activity have peak pedestrian volumes of 400 pedestrians/peak 15-minute period, as per Table 6.3.1. in the TAC *Geometric Design Guide for Canadian Roads*.

1. Table C-5 from *Birtish Columbia Active Transportation Design Guide 2019*.

Letdown ratings are as follows.

Good:

- Letdowns are aligned with crosswalk,
- has tactile pads,
- no tripping hazards.

Fair:

- Letdowns are aligned with crosswalk.
- Crosswalks that are not aligned with crosswalks but are in low foot traffic areas.
- No tactile pad but have concrete grooves.
- Stairs in sidewalk with accessibility signage.

Poor:

- Letdowns are not aligned with crosswalk and in medium to high foot traffic areas.
- Excessive curb lip.
- Poses risk to public through material defects and lacking adequate accessibility features.
- Stairs in sidewalk with no accommodation to accessibility.

4.0 ASPHALT CONCRETE CONDITIONS

4.1 FLEXIBLE PAVEMENTS (AC)

This section covers the pavement distresses that occur on asphalt concrete (AC) surfaced pavements, including AC overlays on either asphalt concrete or Portland cement concrete (PCC) pavements.

This section discusses the criteria required to identify the type, severity, and extent of each of the thirteen distress types that can be inventoried on flexible (asphalt concrete) surfaced pavements. The distress types covered in this section are:

- Fatigue (Alligator) Cracking,
- Progressive Edge Cracking,
- Block Cracking,
- Longitudinal Cracking,
- Transverse Cracking,
- Wheel Track Rutting,
- Shoving (Rippling),
- Distortion,
- Excessive Crown,
- Potholes,
- Raveling,
- Bleeding,
- Patching.

These distress types are categorized into four (4) main distress classifications. The distress classification method groups individual pavement distresses by similar or common defect causes. The four main distress classifications are listed below:

- Load Associated Distresses [Class 1],
- Non-Load Associated Distresses [Class 2],
- Surface Deformations [Class 3],
- Surface Defects [Class 4].

Table 4.1 summarizes the primary distress causes.

Table 4.1: Possible Cause of Distresses

| Problem | Structural Failure | Mix Composition | Temperature or Moisture Changes | Construction |
|--|--------------------|-----------------|---------------------------------|--------------|
| Surface Defects: (Class 4) | | X | X | X |
| Raveling (Class 4) | | X | | X |
| Bleeding (Class 4) | | X | X | X |
| Potholes (Class 4) | | X | X | X |
| Surface Deformations: (Class 3) | X | | | X |
| Rutting (Class 3) | X | X | | X |
| Rippling (Class 3) | X | X | | X |
| Depressions (Class 3) | X | | | X |
| Upheaval (Class 3) | | | X | |
| Slippage (Class 3) | | | | X |
| Edge Lipping (Class 3) | X | | | X |
| Excessive Crown (Class 3) | X | | | X |
| Cracking: (Classes 1 and 2) | X | X | X | |
| Alligator (Class 1) | X | | | |
| Longitudinal (Class 2) | | X | X | |
| Meandering (Class 1) | X | X | X | |
| Transverse (Class 2) | | X | X | |
| Edge Cracking (Class 1) | | X | X | |
| Block Cracking (Class 2) | X | X | X | |

4.2 LOAD ASSOCIATED DISTRESSES

The Load Associated distress types include Fatigue (Alligator) cracking and Progressive Edge cracking.

4.3 FATIGUE CRACKING

Description:

- Occurs in areas subjected to repeated traffic loading (wheel paths),
- Can be a series of interconnected cracks, in early stages of development,
- Develops into many-sided, sharp-angled pieces, usually less than 0.3 m (1.0 ft) on the longest side,
- Develops a characteristic alligator pattern in later stages of deterioration,
- Must have a definable area.

Alligator Cracking

Pattern cracking that occurs in areas of localized load related distress, independent of the wheel paths.

Longitudinal Wheel Path Cracking

Longitudinal cracks with associated random cracking, which occur in the wheel path, are identified as Fatigue Cracking.

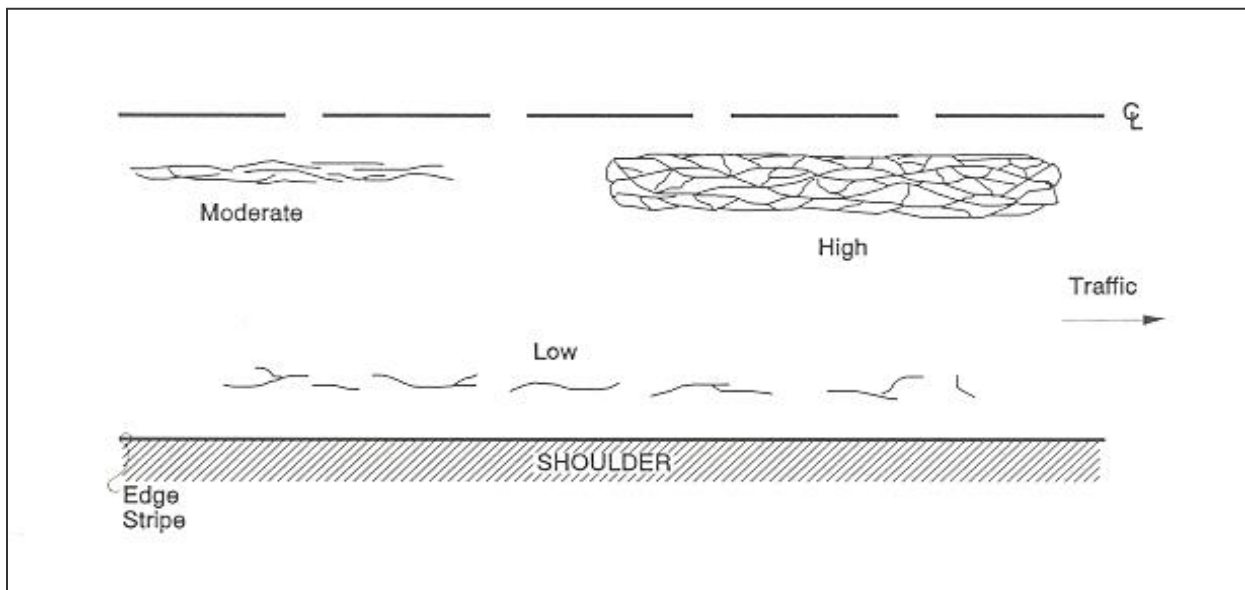


Figure 4.1: Fatigue (Alligator) Cracking Diagram

Severity Level

Severity based on crack condition.

Light

An area of cracks with no or only a few connecting cracks; cracks are not spalled; cracks may be sealed; pumping is not evident. Longitudinal cracks WITH associated random cracking, which occur in the wheel path.

Moderate

An area of interconnected cracks forming a complete pattern; cracks may be slightly spalled; pumping is not evident.

Severe

An area of moderately or severely spalled interconnected cracks forming a complete pattern; pieces may be loose or missing; pumping may be evident.

Special Rule

An area of cracking that spans continuously from the pavement edge into the wheel path area shall be identified as Fatigue (Alligator) cracking.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.



Figure 4.2: Slight Alligator Cracking (Image used for illustration)



Figure 4.3: Moderate Alligator Cracking (Image used for illustration)



Figure 4.4: Severe Fatigue (Alligator) Cracking (Image used for illustration)

4.4 PROGRESSIVE EDGE CRACKING

Description:

- Crescent-shaped cracks or fairly continuous cracks which intersect the pavement edge and are located within the edge-area, within 0.6 m (2 ft) of the pavement edge,
- Progressive Edge cracking will gradually encroach into the outer wheel path,
- Areas of isolated edge break-off that can occur independent of associated cracking.

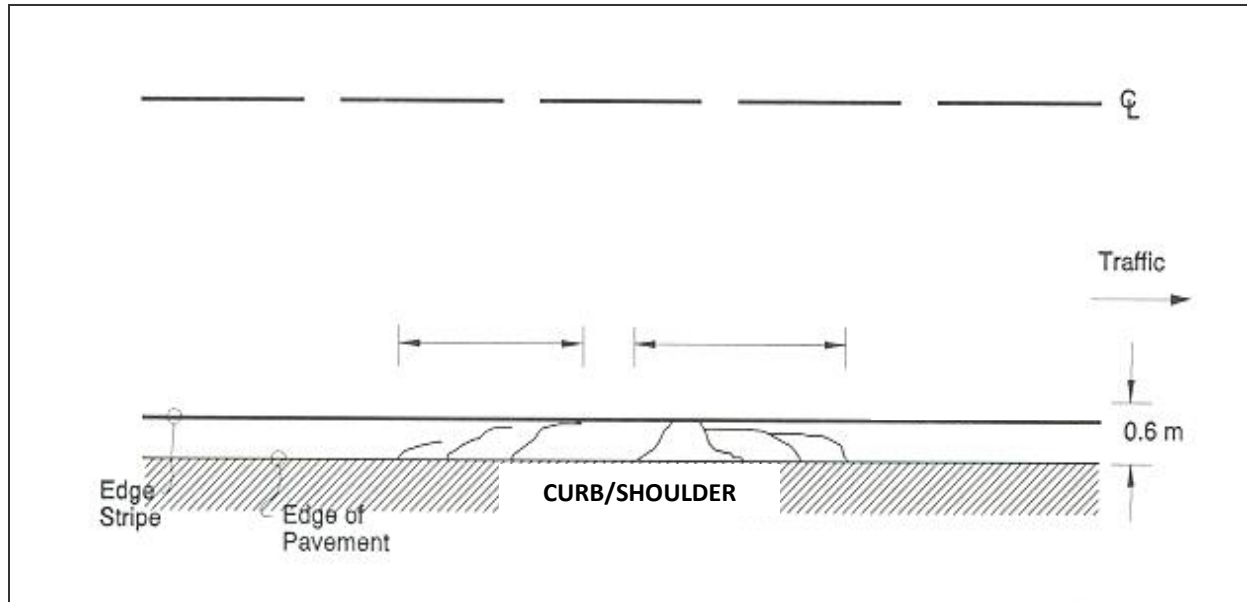


Figure 4.5: Progressive Edge Cracking Diagram

Severity Level

Severity level is based on condition and distance, meters (feet), the crack has encroached into the pavement from the pavement edge (random or alligator formation).

Low

Cracking encroachment <0.3 m (1 ft) from the pavement edge, single or multiple cracks, crescent shaped wave formation cracking.

Moderate

Cracking encroachment between 0.3 m (1 ft) to 0.6 m (2 ft) from pavement edge with multiple spalled cracking, or multiple interconnecting cracks; no edge break-up occurs.

High

Cracking encroachment >0.6 m (2 ft) from pavement edge that has not reached the outer wheel path; alligator of pavement occurs at/or near the edge of pavement; pavement edge is broken up and lost.

Special Rule

An area of cracking that spans continuously from the pavement edge into the outer wheel path area, regardless of encroachment distance, shall NOT be identified as Progressive Edge cracking; see *Fatigue (Alligator) Cracking*.

Transverse cracks exhibiting associated multiple cracking that forms a complete interconnected pattern with the pavement edge (delta pattern), which occurs in the edge-area, 0.6 m (2 ft) of the pavement edge, shall be identified as Progressive Edge Cracking, using the edge cracking criteria. No transverse cracking is recorded in the edge-area for this condition.

Extent

Record the length, meters (feet), of affected pavement edge at each severity level. If the different severity levels existing within an affected length cannot be distinguished, rate the entire length at the highest severity level.



Figure 4.6: Slight Edge Cracking (Image used for illustration)



Figure 4.7: Moderate Edge Cracking (Image used for illustration)



Figure 4.8: Severe Edge Cracking (Image used for illustration)

4.5 NON-LOAD ASSOCIATED CRACKING

The Non-Load Associated distress types, often referred to as Environmental cracking, include, Block (Map) cracking, Longitudinal cracking and Transverse cracking.

4.6 BLOCK CRACKING

Description:

- A pattern of cracks that divides the pavement into approximately rectangular pieces (Blocks),
- Blocks range in size from approximately 0.1 m² (1 ft²) to 10 m² (100 ft²), with minimum plan dimensions ranging in size from 0.3 m (1 ft) to 3 m (10 ft).

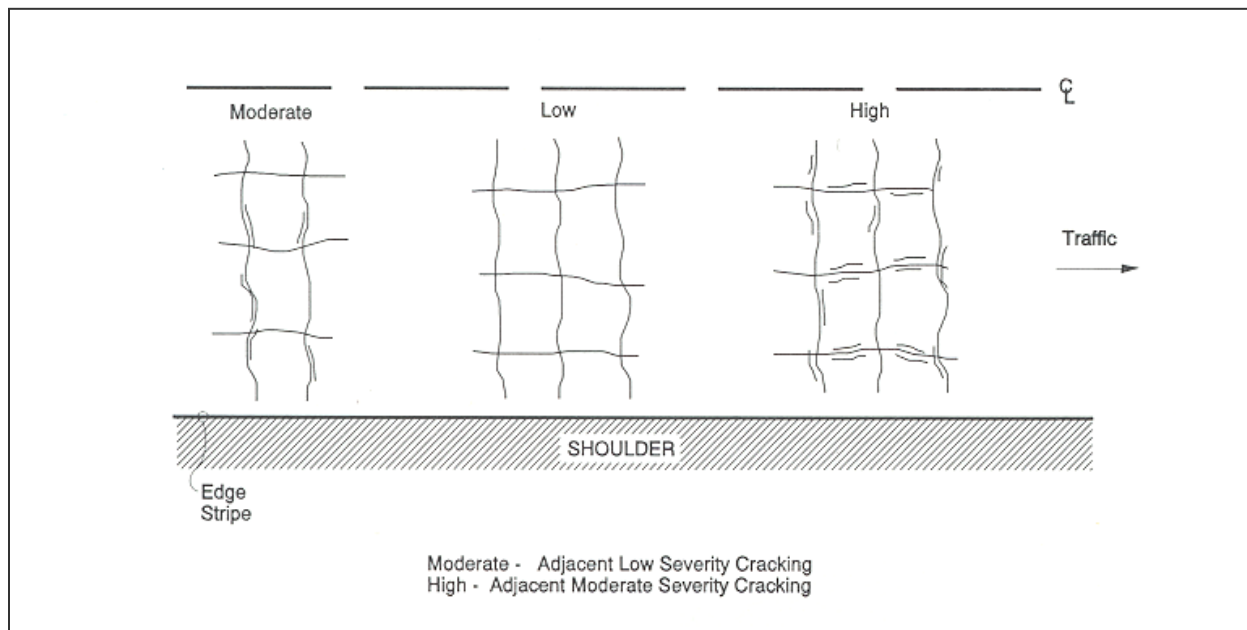


Figure 4.9: Block/Map Cracking Diagram

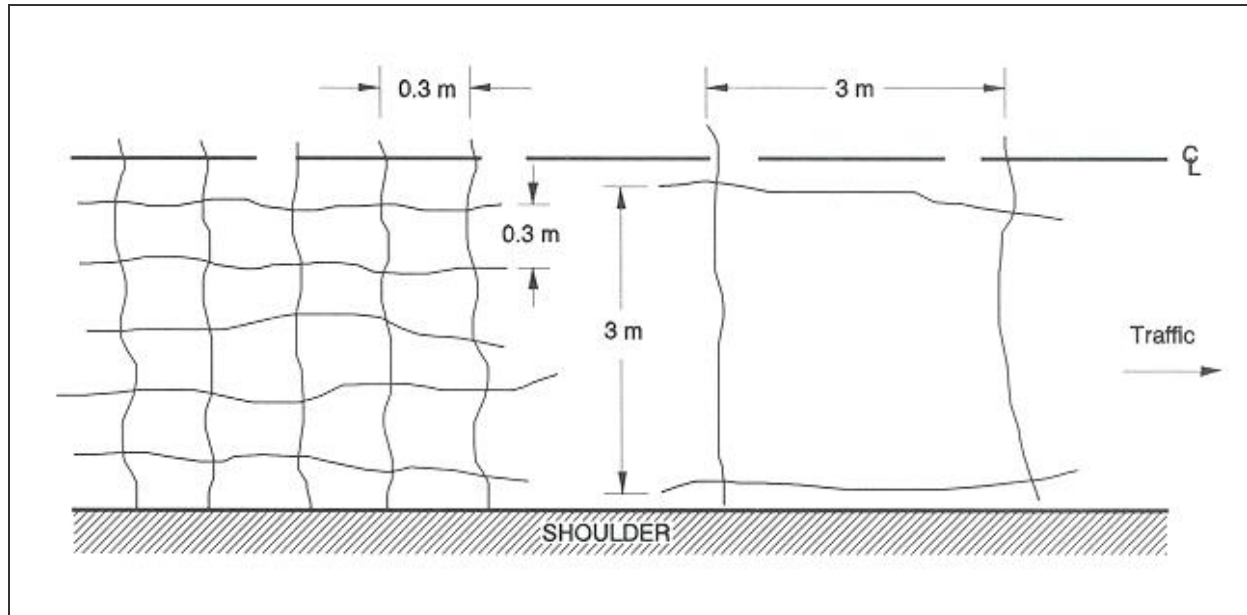


Figure 4.10: Block/Map Cracking Dimensions Diagram

Severity Level

Severity based on crack width and condition.

Light

<13 mm (0.5 in) wide or sealed crack with sealant material in good condition; single crack with NO associated random cracking.

Moderate

13 mm to 75 mm (0.5 in to 3 in); single crack or with associated random cracking that has not yet formed an interconnected pattern.

Severe

>75 mm (3 in); single crack or multiple cracking forming a complete interconnected pattern resembling alligator cracking; spalling beginning to develop.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.

If Fatigue (Alligator) cracking exists within the Block cracking area, the area of Block cracking area recorded is reduced by the area of Fatigue cracking. The Fatigue cracking area is recorded separately, based on the Fatigue cracking criteria.

Special Rule

An occurrence of Block cracking should be at least 15 m (50 ft) long before being classified as block cracking. An occurrence less than 15 m (50 ft) shall be rated as a combination of Longitudinal and Transverse cracks, based on the respective criteria.

Block cracking which exhibits severe characteristics due to associated multiple cracking that forms a complete interconnected pattern, which has a width across each crack axis greater than 0.6 m (2 ft), shall be recorded as Fatigue (Alligator) cracking, using the Fatigue cracking criteria. No Block cracking shall be recorded for this advanced condition; the Fatigue cracking 'condition' has replaced the Block cracking 'condition'.



Figure 4.11: Slight Block/Map Cracking (Image used for illustration)



Figure 4.12: Moderate Block/Map Cracking (Image used for illustration)



Figure 4.13: Block/Map Cracking (Image used for illustration)

4.7 LONGITUDINAL CRACKING

Description:

- Cracks predominantly parallel to pavement centerline. Location within the lane (wheel path versus non-wheel path) is significant,
- Open seams along patch edges, running parallel to the pavement centerline.

Meandering Crack

Crack running diagonally from edge to edge of pavement, usually of quite long lengths.

Reflection Crack

Longitudinal cracks in asphalt concrete overlay surfaces that occur over longitudinal joints in concrete (PCC) pavements.

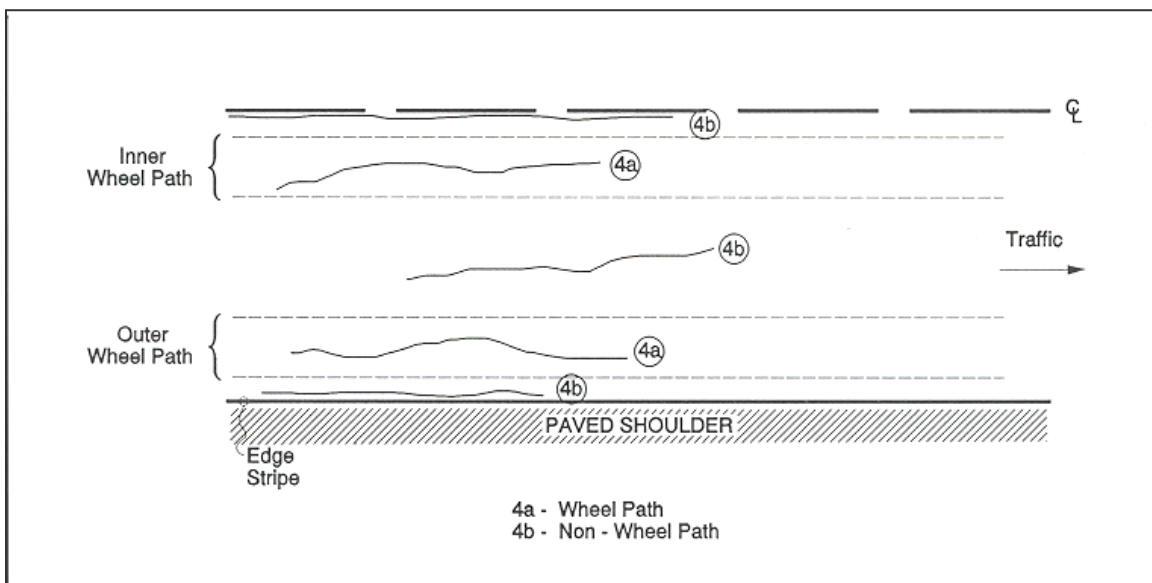


Figure 4.14: Longitudinal Cracking Diagram

Severity Level

Severity based on crack width and condition.

Light

<13 mm (0.5 in) wide or sealed crack with sealant material in good condition; single crack with NO associated random cracking.

Moderate

13-75 mm (0.5-3 in); single crack or with associated random cracking that has not yet formed an interconnected pattern.

Severe

>75 mm (3 in); single crack or multiple cracking forming a complete interconnected pattern resembling alligator cracking; spalling beginning to develop.

Special Rule

Any longitudinal crack that occurs in the wheel paths and has associated random cracking, forming a quantifiable area, is considered LOW severity Fatigue cracking. All further deterioration of this condition of wheel path longitudinal cracking shall be recorded as Fatigue cracking using the Fatigue cracking criteria.

Any Longitudinal crack that occurs outside of the wheel paths and exhibits severe characteristics due to associated multiple cracking that forms a complete interconnected pattern, which has a width across the crack greater than 0.6 m (2 ft), shall be rated as Fatigue cracking, using the Fatigue cracking criteria. No Longitudinal cracking shall be recorded for this advanced condition; the Fatigue cracking 'condition' has replaced the Longitudinal cracking 'condition'.

Extent

Record meters (feet) of affected length at each severity level. If the different severity levels existing within an affected length cannot be distinguished, rate the entire length at the highest severity level.

Record total length of cracking that occurs in the wheel paths separately from the length of cracking that occurs outside of the wheel paths.



Figure 4.15: Moderate Longitudinal Cracking (Image used for illustration)



Figure 4.16: Moderate Longitudinal Cracking (Occurs in Wheel Path – rated as Fatigue Cracking) (Image used for illustration)



Figure 4.17: Severe Longitudinal Cracking (Image used for illustration)

4.8 TRANSVERSE CRACKING

Description:

- Cracks predominantly perpendicular to pavement centerline,
- Full Transverse cracks tend to be regularly spaced along the length of the road; half and partial Transverse cracks occur at shorter intermediate distance,
- Open seams along patch edges, running perpendicular to the pavement centerline.

4.9 REFLECTION CRACK

Transverse cracks in asphalt concrete overlay surfaces that occur over transverse joints in concrete (PCC) pavements.

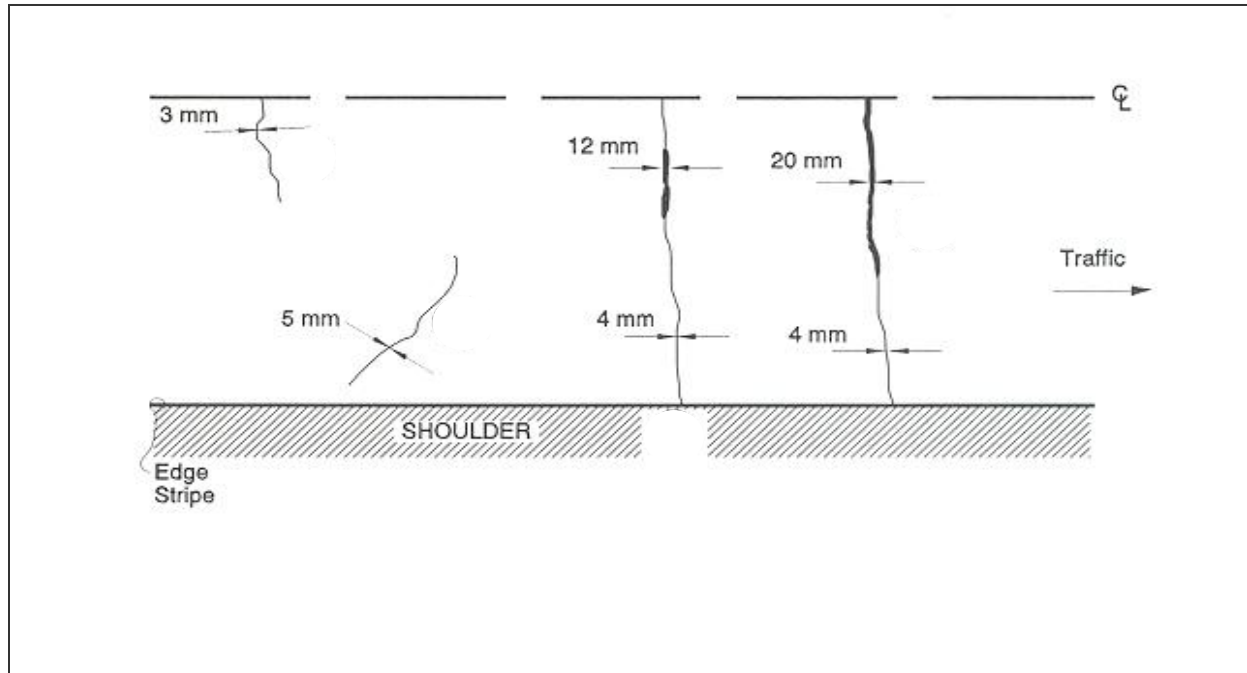


Figure 4.18: Transverse Cracking Diagram

Severity Level

Severity based on crack width and condition.

Low

<13 mm (0.5 in) wide or sealed crack with sealant material in good condition; single crack with NO associated random cracking.

Moderate

13-75 mm (0.5-3 in); single crack or with associated random cracking that has not yet formed an interconnected pattern.

Severe

>75 mm (3 in); single crack or multiple cracking forming a complete interconnected pattern resembling alligator cracking; spalling beginning to develop.

Special Rule

Any Transverse crack that exhibits severe characteristics due to associated multiple cracking that forms a complete interconnected pattern, which has a width across the crack greater than 0.6 m (2 ft), shall be rated as Fatigue (Alligator) cracking, using the Fatigue cracking criteria. No Transverse cracking shall be

recorded for this advanced condition; the Fatigue cracking 'condition' has replaced the Transverse cracking 'condition'.

Where Transverse cracks exhibit associated multiple cracking that forms a complete interconnected pattern with the pavement edge (delta pattern) and occurs in the edge-area, 0.6 m (2 ft) of the pavement edge, shall be recorded as Progressive Edge cracking, using the Progressive Edge cracking criteria. No Transverse cracking is recorded in the edge-area, for this condition.

Extent

Record meters (feet) of affected length at each severity level. If the different severity levels existing within an affected length cannot be distinguished, rate the entire length at the highest severity level.



Figure 4.19: Moderate Transverse Crack (with random cracking) (Image used for illustration)



Figure 4.20: Moderate Transverse Crack (Image used for illustration)



Figure 4.21: Severe Transverse Crack (Image used for illustration)

4.10 SURFACE DEFORMATIONS

The Surface Deformation distress types, include, Wheel Track Rutting, Rippling (Shoving), Distortions and Excessive Crown.

4.11 WHEEL TRACK RUTTING

Description:

- A rut is a longitudinal depression in the wheel path caused by traffic loading. It may have associated transverse displacement.
- In extreme loading and environmental conditions dual-track ruts can occur in the wheel paths.

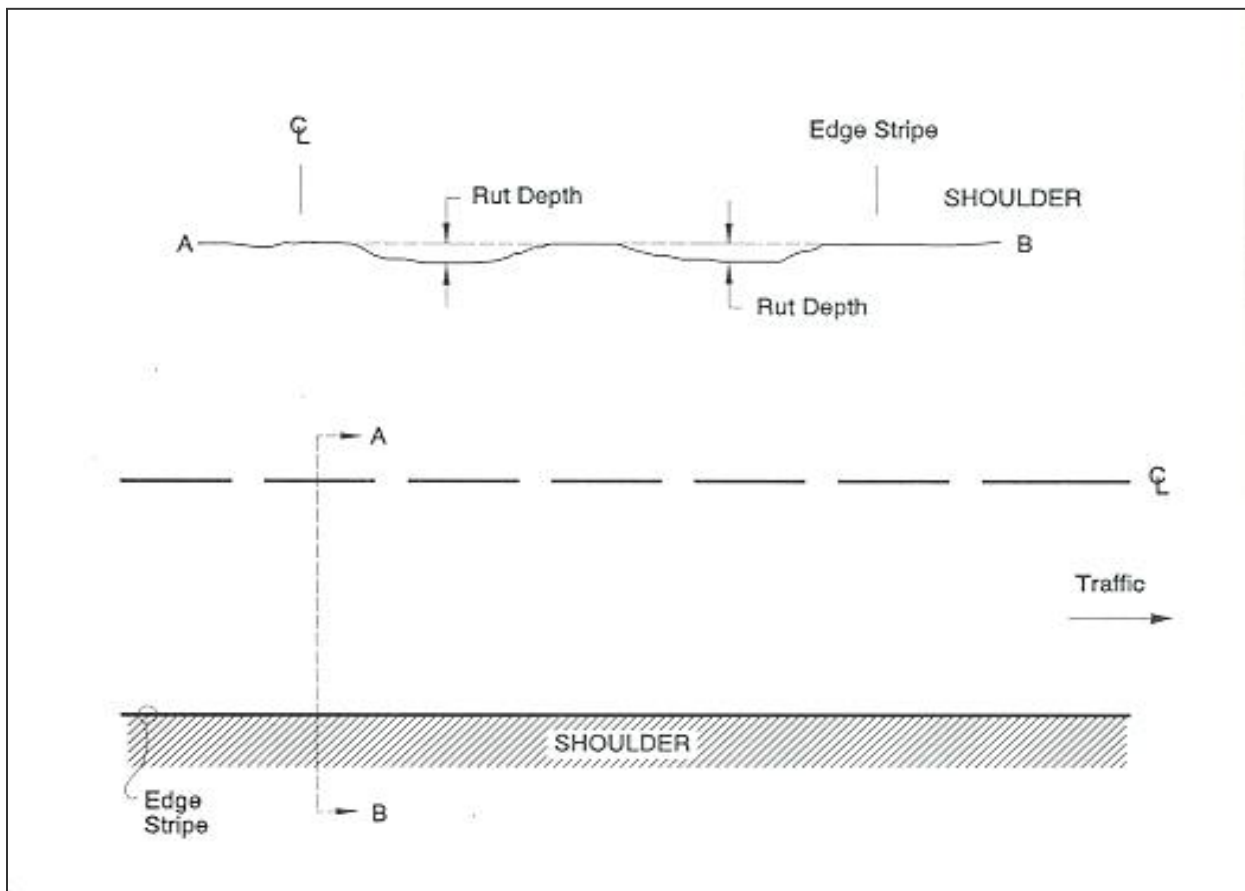


Figure 4.22: Wheel Track Rutting Diagram

Severity Level

Severity is based on rut depth.

Light

Rut depth 13-25 mm (0.5-1 in)

Moderate

Rut depth 25-50 mm (1-2 in)

Severe

Rut depth >50 mm (2 in)

Special Rule

Measurements will be taken in both inner and outer wheel paths. If rutting depth varies from the inner to outer wheel path, record the most severe condition for length of rutting observed.

Extent

Record meters (feet) of affected length at each severity level. If the different severity levels existing within an affected length cannot be distinguished, rate the entire length at the highest severity level.



Figure 4.23: Slight Wheel Track Rutting (Image used for illustration)



Figure 4.24: Moderate Wheel Track Rutting (Image used for illustration)



Figure 4.25: Severe Wheel Track Rutting (Image used for illustration)

4.12 SHOVING (RIPPLING)

Description:

- Shoving is a longitudinal displacement of a localized area of the pavement surface in the wheel paths.
- Shoving is either a sideways movement of asphalt, or a volume pushed transversely to wheel path.
- It is generally caused by braking or accelerating vehicles, and is usually located on hills or curves, or at intersections.

Rippling

The vertical displacements of the pavement surface occurring in the area affected by Shoving. It forms regular undulations, similar to a 'washboard' effect, on the pavement surface and affects the rideability of the pavement surface.

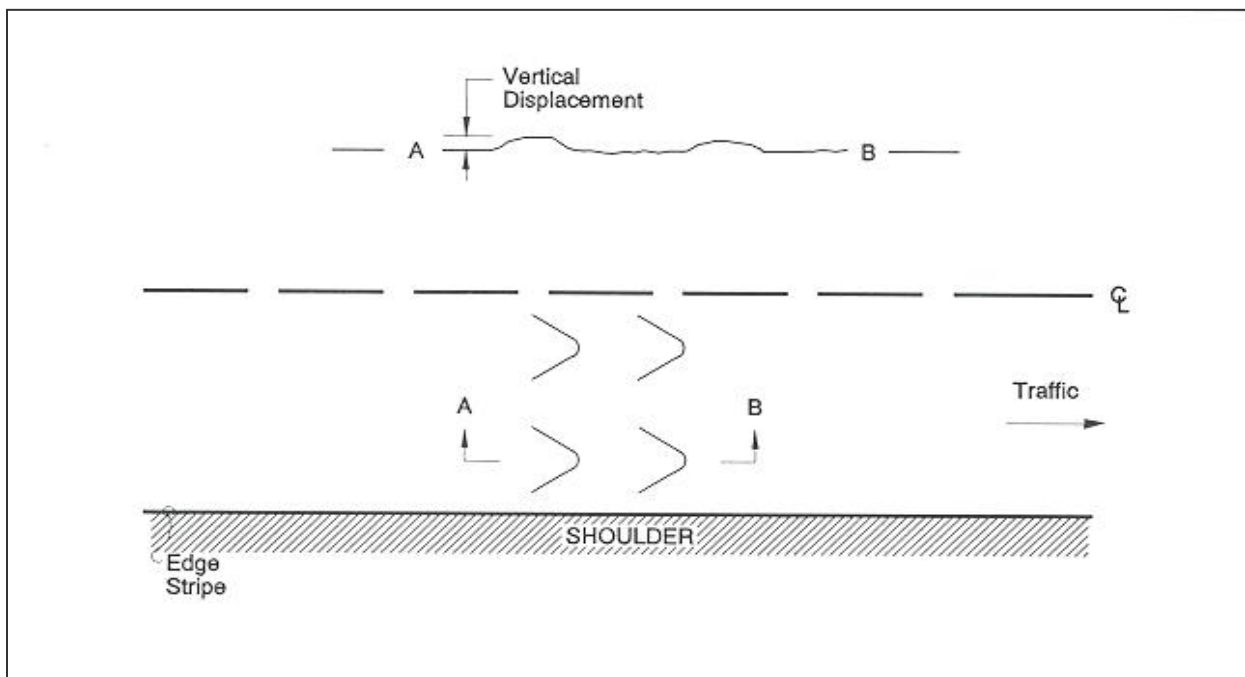


Figure 4.26: Shoving (Rippling) Diagram

Severity Level

Severity is based on rideability of the pavement surface.

Light

Noticeable decrease in rideability.

Moderate

Rough ride.

Severe

Very rough ride; may require the vehicle to reduce speed.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.



Figure 4.27: Slight Shoving (Rippling) (Image used for illustration)



Figure 4.28: Moderate Shoving (Rippling) (Image used for illustration)



Figure 4.29: Severe Shoving (Rippling) (Image used for illustration)

4.13 DISTORTION

Description:

- Any deviation of the pavement surface from its original shape is classified as distortion,
- These surface deformations may take the form of dishing, bumps, dips, swelling and sags,
- Distortion does not include Wheel Ruts, areas of Shoving and Rippling, or Potholes.

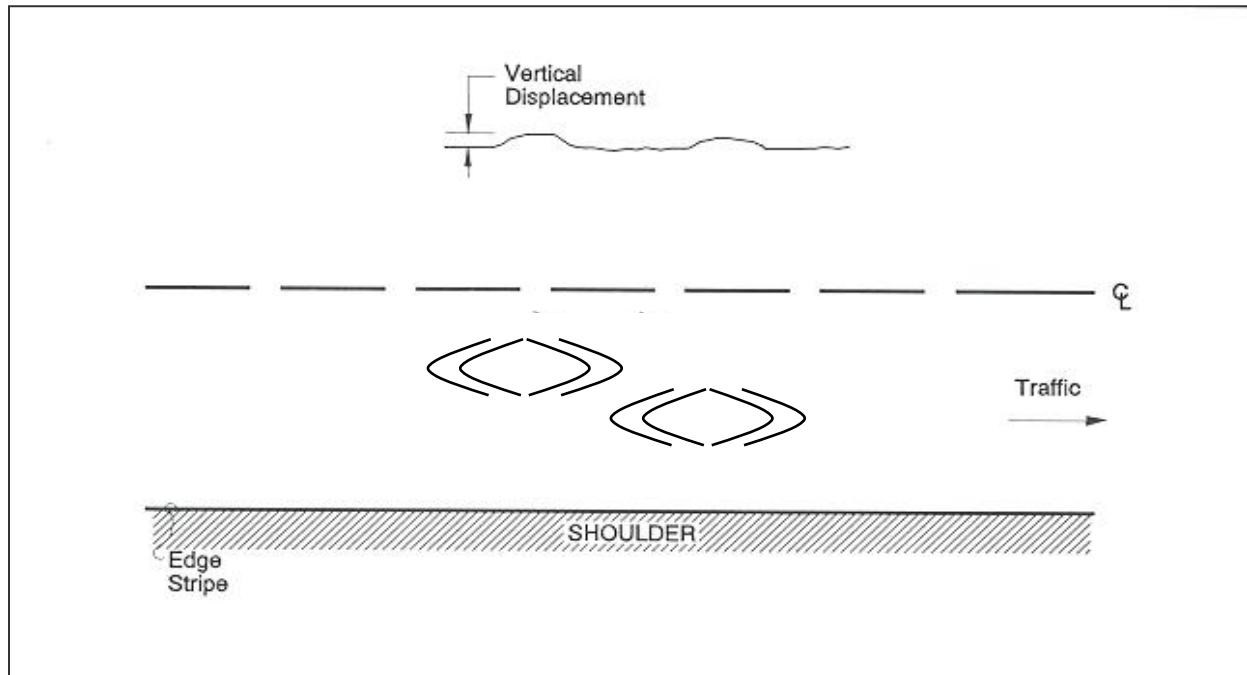


Figure 4.30: Distortion Diagram

Severity Level

Severity is based on vertical displacement and length of affected area.

Light

Rise or depression which is less than 3 m (10 ft) in length and less than 50 mm (2 in) in height deviation from its normal plane. Generally speaking, slight corrugations and dishing will be in the low level distortion.

Moderate

Rise or depression which is between 50-100 mm (2-4 in) in deviation from its normal plane. Some types of distortion to look for are: bumps, sags, frost heaves, swelling at pavement edge or slippage.

Severe

Rise or depression which is greater than 100 mm (4 in) in deviation from its normal plane. It can include all the previously indicated distress types plus swelling and settlements which cause large and/or long dips in the pavement.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.



Figure 4.31: Slight Distortion (Image used for illustration)



Figure 4.32: Moderate Distortion (Image used for illustration)



Figure 4.33: Severe Distortion (Image used for illustration)

4.14 EXCESSIVE CROWN

Description:

- This condition is characterized by: (A) a difference in elevation between the longitudinal pavement edge and the shoulder material, or (B) the mid-lane to edge of pavement cross section has tilted.
- Can be a result of successive asphalt overlays that were tapered to meet the curb-gutter which have built up or 'crowned' the pavement center.

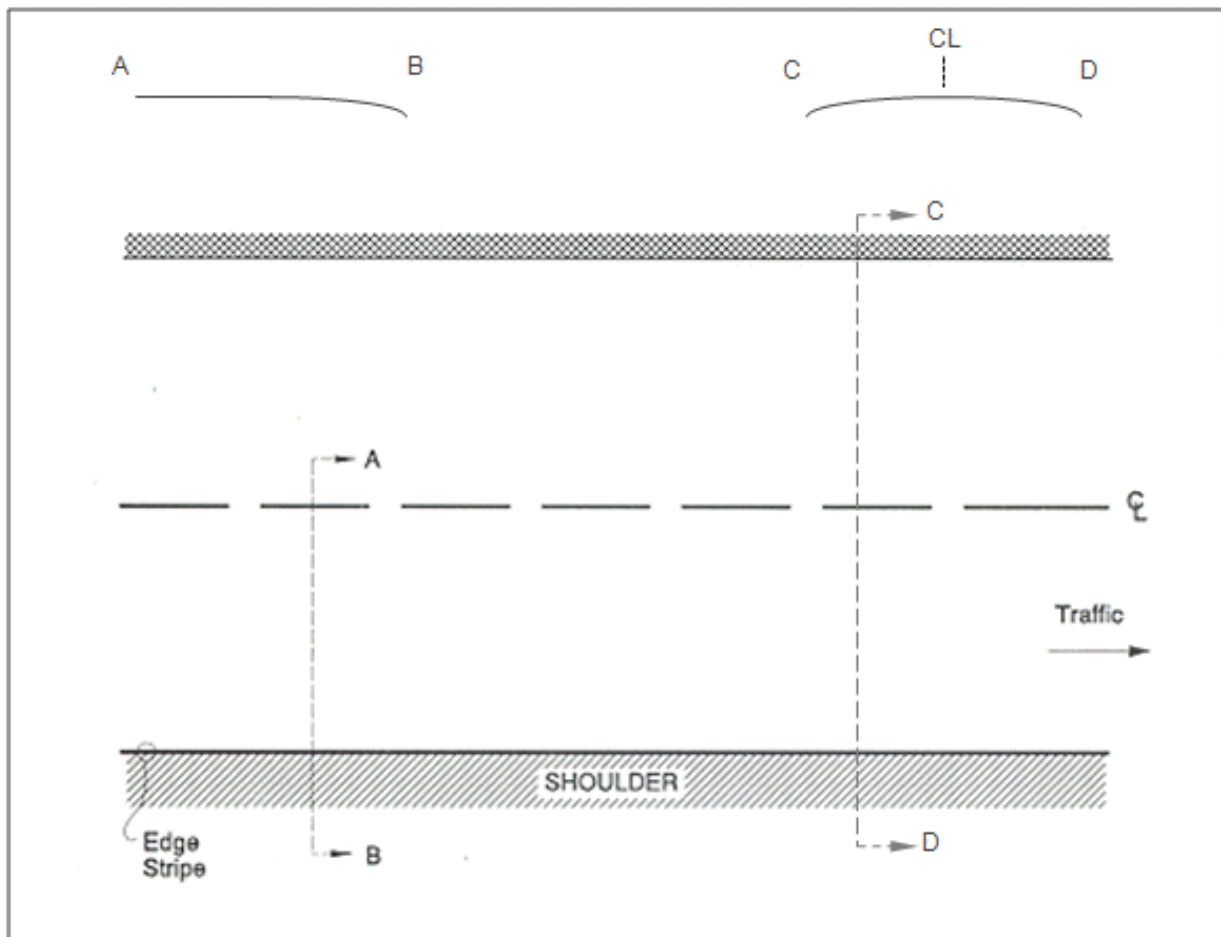


Figure 4.34: Excessive Crown Diagram

Severity Level

Severity is based on transverse cross slope and ability to control the vehicle.

Light

3.0-3.9% cross slope from centerline to pavement edge. Noticeable tilting of vehicle with good control.

Moderate

4.0-5.9% cross slope from centerline to pavement edge. Moderate tilting of vehicle with fair control.

High

> 6.0% cross slope from centerline to pavement edge. Excessive tilting of vehicle with poor control.

Special Rule

Any occurrence of Excessive Crown should be recorded as affecting either half or the full pavement cross-section, for the given centerline length of the defect.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.

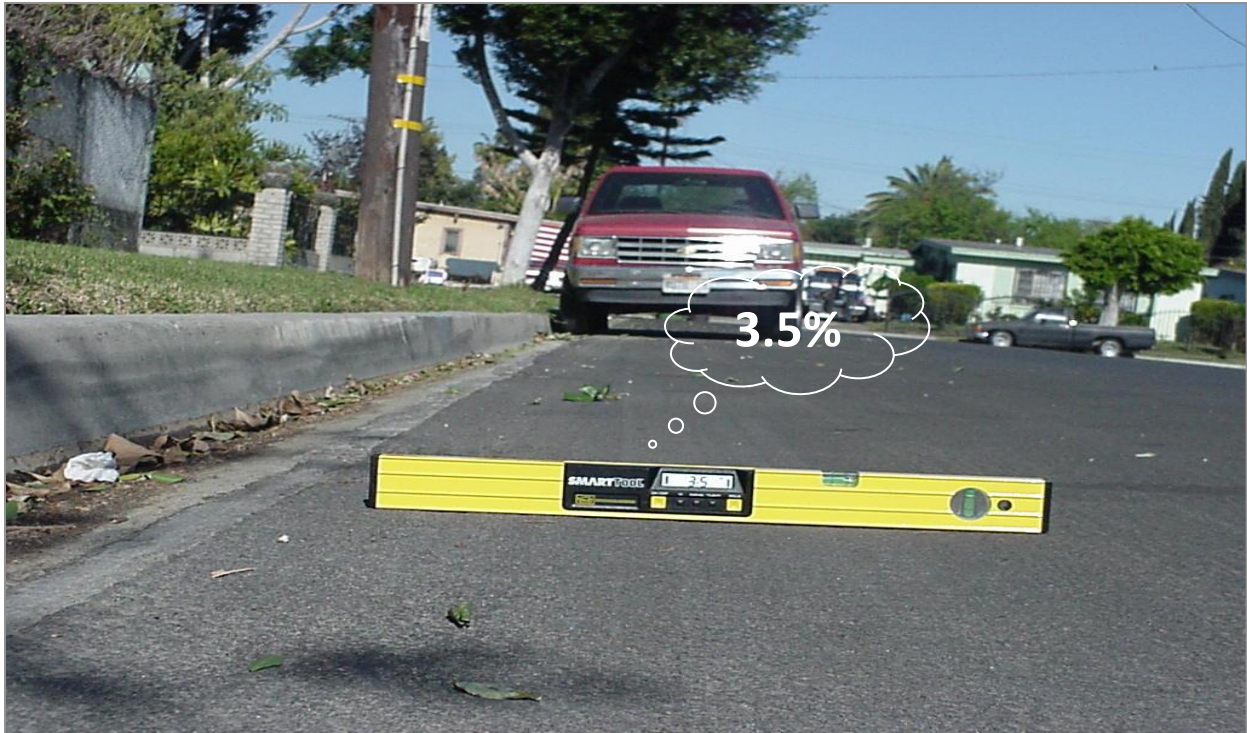


Figure 4.35: Slight Excessive Crown (Image used for illustration)



Figure 4.36: Moderate Excessive Crown (Image used for illustration)



Figure 4.37: Severe Excessive Crown (Image used for illustration)

4.15 SURFACE DEFECTS

The Surface Defect distress types, include, Potholes, Raveling, Bleeding and Patching.

4.16 POTHOLES

Description:

- Bowl-shaped holes of various sizes in the pavement surface,
- Potholes originate at any location in the pavement surface and may be independent of, or contained within, other distresses such as Fatigue (Alligator) cracking, Raveling and Wheel Rutting,
- Generally, Potholes are demand maintenance items, which are addressed on a priority basis.

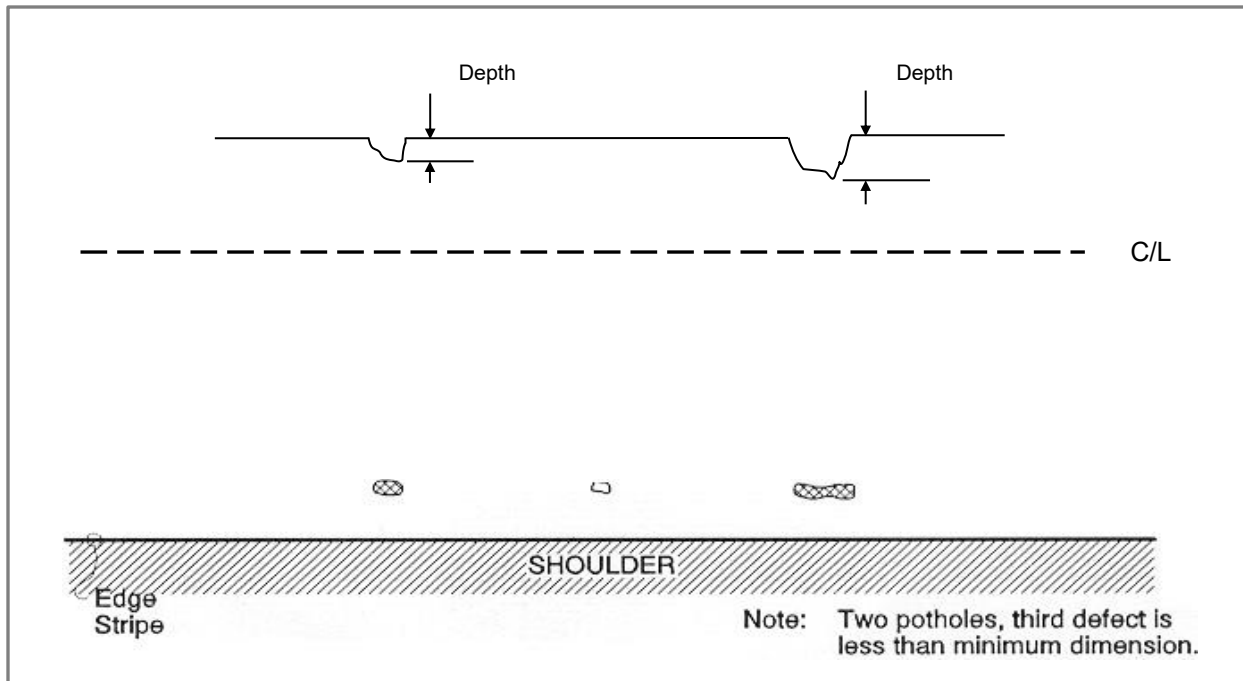


Figure 4.38: Potholes Diagram

Severity Level

Severity is based on plan size and depth of defect.

Light

<75 mm (3 in) wide, shallow surface popouts, <25 mm (1 in) deep.

Moderate

75-300 mm (3-12 in) wide, no base material exposed, 25-50 mm (1-2 in) deep.

Severe

>300 mm (12 in) wide, visible base material, >50 mm (2 in) deep.

Special Rule

Potholes should be recorded in all occurrences, including when they occur within the area affected by other distresses.

Potholes that have been filled will be recorded as Patching.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.



Figure 4.39: Slight Pothole (Image used for illustration)



Figure 4.40: Moderate Pothole (Image used for illustration)



Figure 4.41: Severe Pothole (Image used for illustration)

4.17 RAVELING

Description:

- Wearing away of the pavement surface in hot mix asphalt concrete pavements,
- Progressive loss of pavement material from surface, resulting in a coarse textural appearance. Stripping of aggregate and binder from the wheel paths (traffic action), which can result in a rut formation,
- Raveling appears as small pock marks in the pavement surface. The size of these pock marks depend on the size of stone in mix.

Weathering

This condition is characterized by the wearing away of the pavement surface caused by the loss of asphalt binder. This condition is normally caused by oxidation of the asphalt binder of the mix, resulting in the loss of aggregate and flexibility.

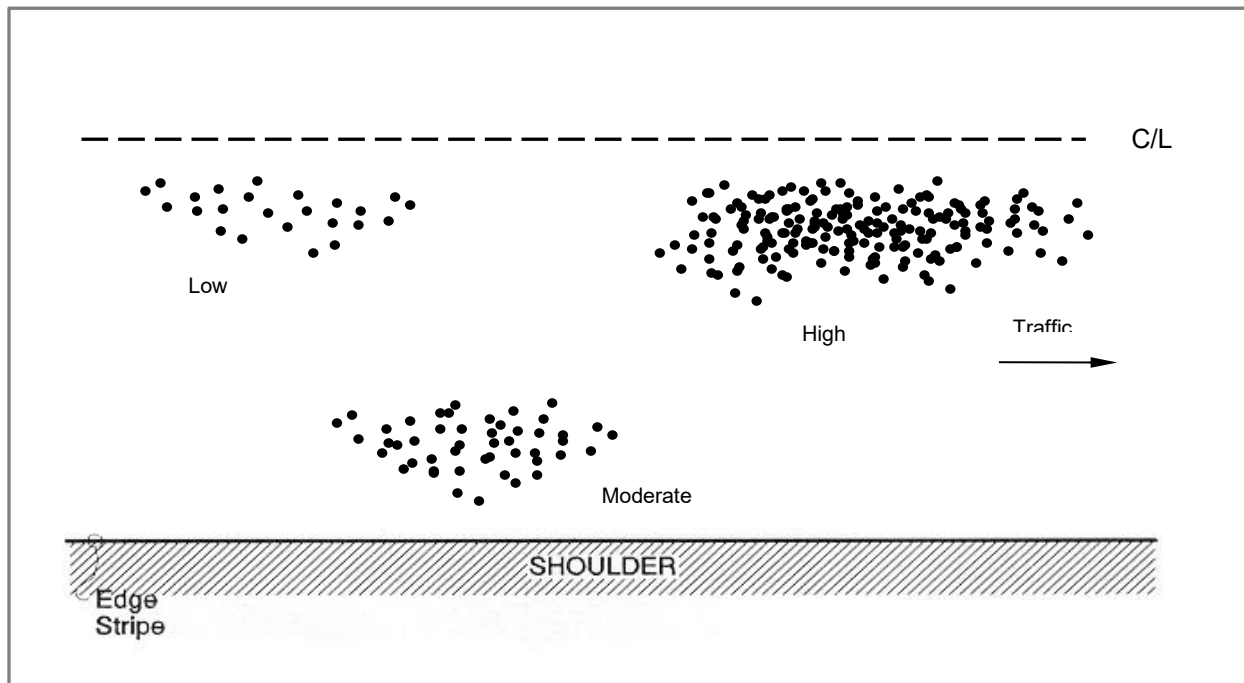


Figure 4.42: Raveling Diagram

Severity Level

Severity is based on condition and appearance.

Light

Noticeable loss of pavement material, with minor loss of surface fines. The vertical faces of the aggregate can be observed.

Moderate

Shallow disintegration with no loss of pavement thickness. The vertical faces of the coarse aggregate are exposed up to half the aggregate size with limited pop-outs of the individual aggregate.

Severe

Noticeable deterioration in pavement thickness due to loss of aggregate. The surface is quite rough and will become noisy with vehicle traffic.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.



Figure 4.43: Slight Raveling (Image used for illustration)



Figure 4.44: Moderate Raveling (Image used for illustration)



Figure 4.45: Severe Raveling (Image used for illustration)

4.18 BLEEDING

Description:

- Excess bituminous binder occurring on the pavement surface.
- May create a shiny, glass-like, reflective surface that may be tacky to the touch.
- Usually found in the wheel paths and is most prevalent in hot climates.

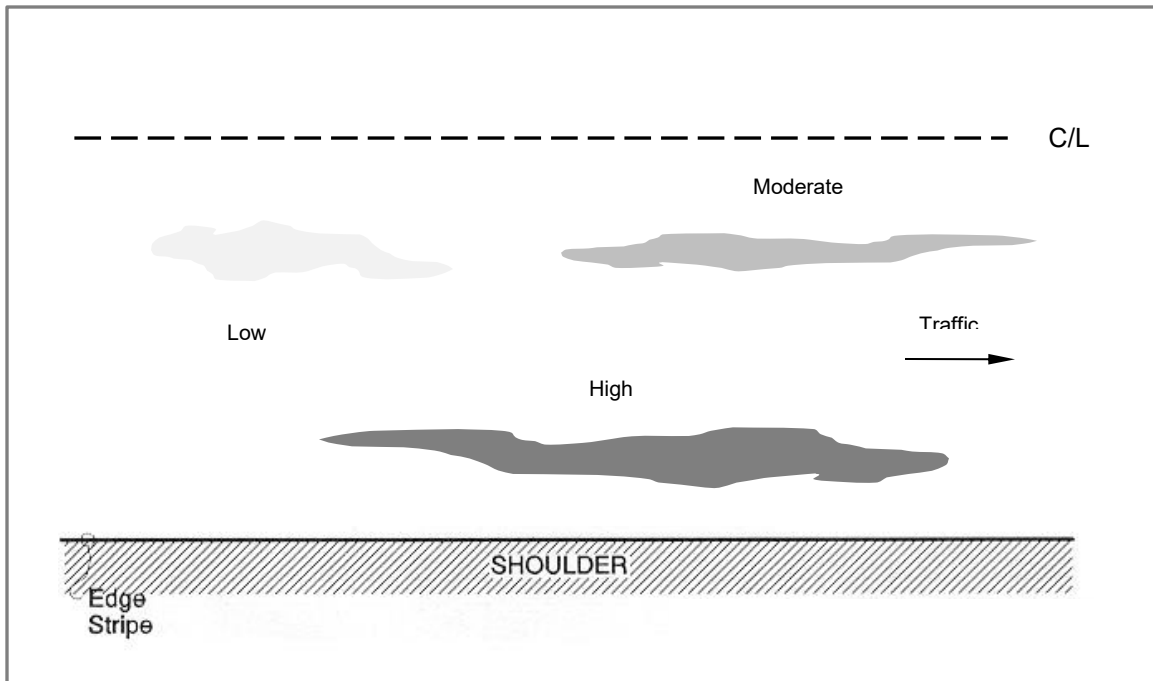


Figure 4.46: Bleeding (Flushing) Diagram

Severity Level

Severity is based on condition and appearance.

Light

Visible coloring or pavement surface (interconnected veining).

Moderate

Distinctive appearance, with excess free asphalt.

Severe

Excessively free asphalt gives the pavement a wet look. Tire marks and tracking are evident, tire noise can be heard.

Special Rule

Care must be taken not to confuse Bleeding with oil impregnated wheel tracks or tire wear in wheel paths.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.

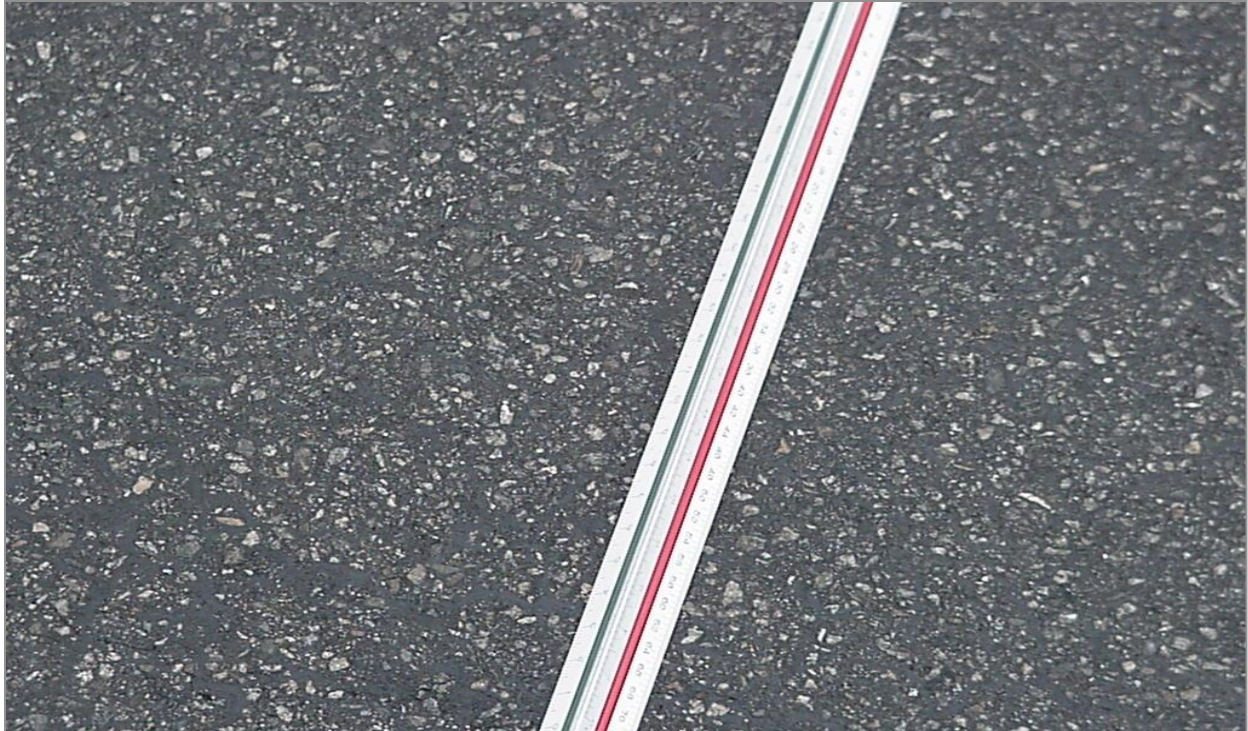


Figure 4.47: Slight Bleeding (Image used for illustration)

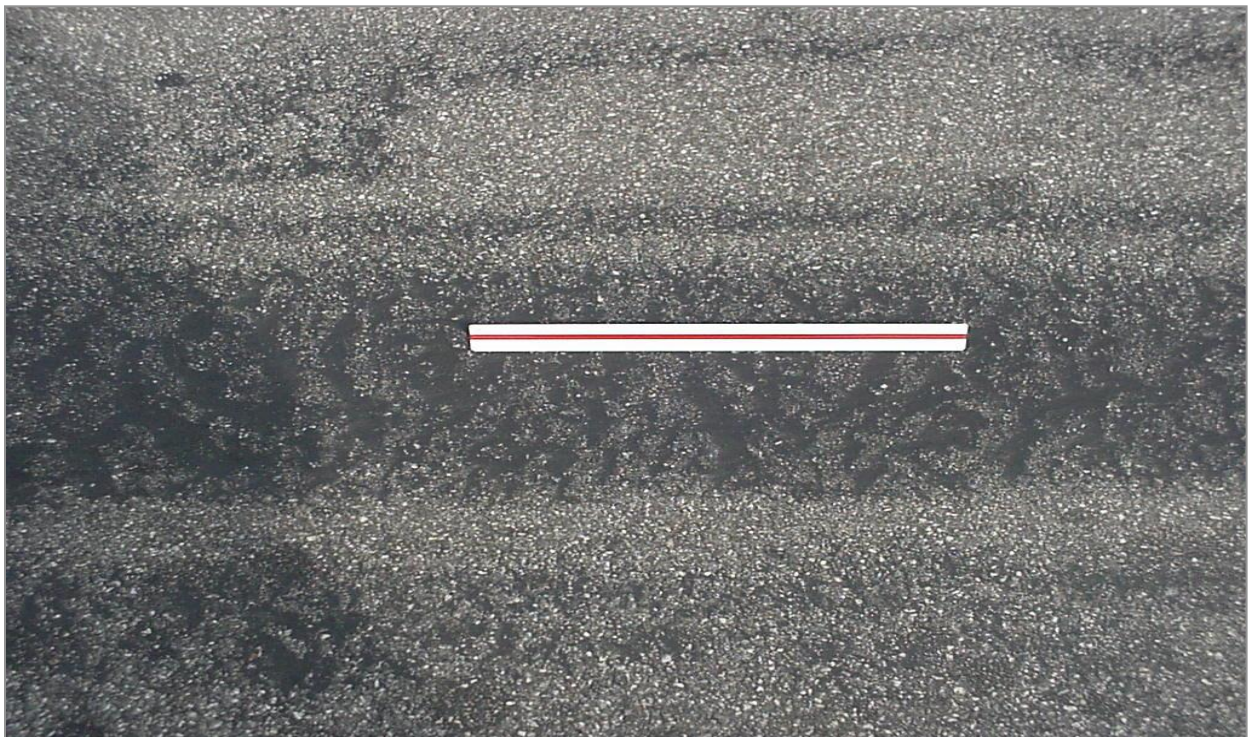


Figure 4.48: Moderate Bleeding (Image used for illustration)



Figure 4.49: Severe Bleeding (Image used for illustration)

4.19 PATCHING

Description:

- Portion of pavement surface that has been removed and replaced or additional material applied to the pavement after original construction,
- Conventional methods may include hot mix, cold mix, spray patch, Portland cement (PCC) concrete, utility cuts, or skin patch.

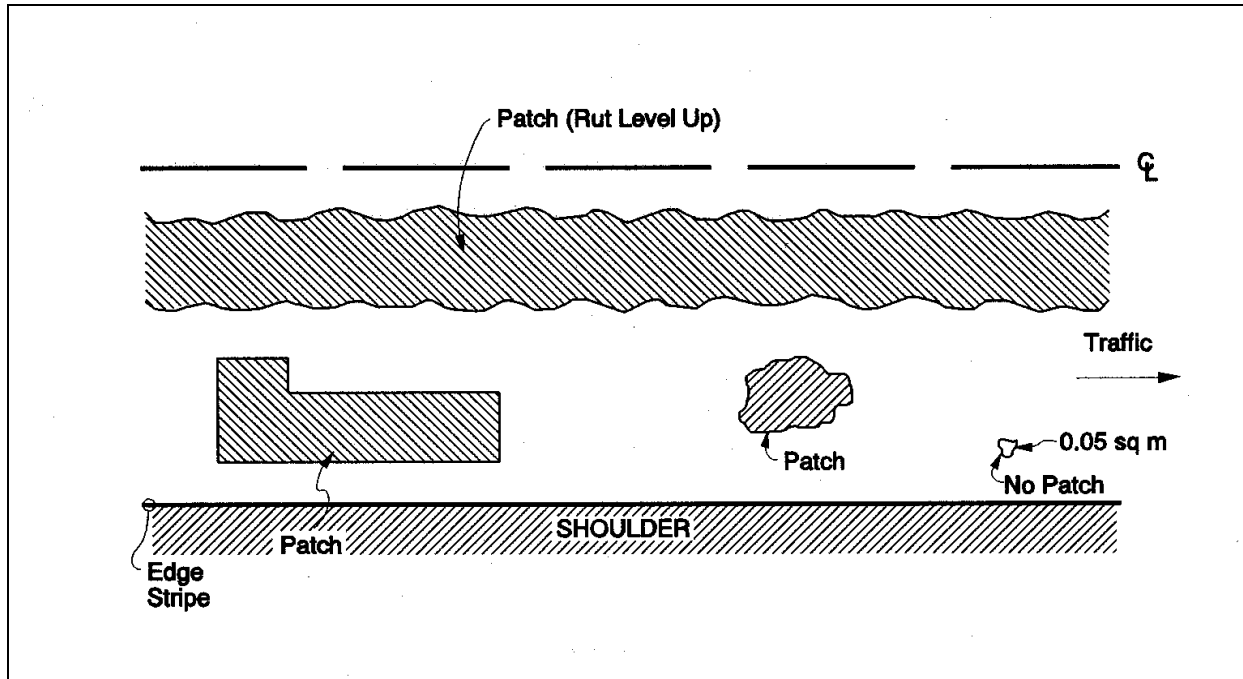


Figure 4.50: Patching Diagram

Severity Level

Severity is based on condition and presence of other distresses that occur within the Patch area.

Light

Patch is in good condition with no other defects occurring within the Patch area.

Moderate

Some deterioration of material. Presence of other defects occurring at the LOW severity level.

Patch seams are starting to open.

Severe

Significant deterioration of material. Presence of other defects occurring at the MODERATE or HIGH severity level. Patch seams are completely opened. Patch needs to be replaced.

Special Rule

Patching is always considered a defect.

Any defect that occurs within the area of a Patch shall be recorded separately based on the identification criteria for that particular defect.

Open seams along the perimeter of a patch are considered cracks and shall be recorded as either Longitudinal or Transverse cracking, depending on the orientation of the affected seam.

Extent

Record square meters (square feet) of affected area at each severity level. If the different severity levels existing within an affected area cannot be distinguished, rate the entire area at the highest severity level.



Figure 4.51: Slight (Good) Patch (Image used for illustration)



Figure 4.52: Moderate (Borderline) Patch (Image used for illustration)



Figure 4.53: Severe (Failed) Patch (Image used for illustration)